BOROUGH OF FLEMINGTON MAIN STREET SIDEWALK & STREETSCAPE BLOOMFIELD AVENUE TO MINE STREET BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY FEDERAL AID PROJECT #TAP-1006(335) STATE JOB #6409314

MAYOR AND COUNCIL MEMBERS

PHIL GREINER, MAYOR

BRIAN SWINGLE, COUNCIL PRESIDENT MARC HAIN, COUNCIL VICE PRESIDENT

JOHN GORMAN, COUNCIL MEMBER BROOKE LIEBOWITZ, COUNCIL MEMBER MICHELLE OBERST, COUNCIL MEMBER AL BREWER, COUNCIL MEMBER

BOROUGH ENGINEER

ROBERT MARTUCCI, P.E., C.M.E.

UTILITY OWNERS LIST:

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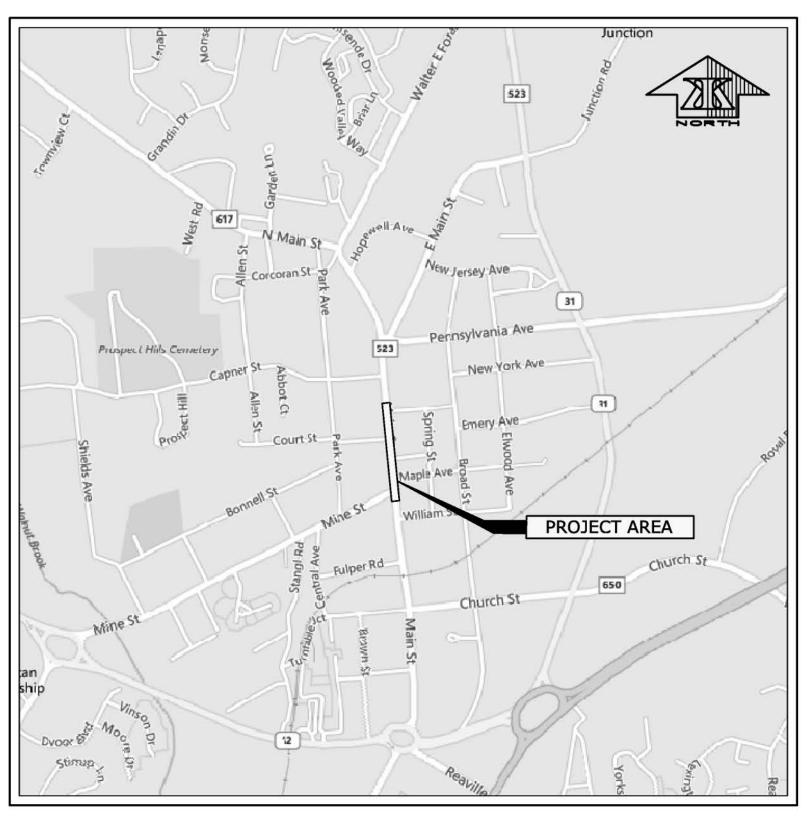
GAS: (TEL.# 908-662-8321) ELIZABETHTOWN GAS COMPANY 520 GREEN LANE UNION, NJ 07083 CONTACT: GREG BALINT

ELECTRIC: (TEL.# 973-401-8521) JERSEY CENTRAL POWER & LIGHT (JCP&L) 300 MADISON AVENUE MORRISTOWN, NJ 07962-0911 CONTACT: FRANK MERCADANTE

SEWER: (TEL.# 908-782-8840) BOROUGH OF FLEMINGTON SEWER DEPT. 38 PARK AVENUE FLEMINGTON, NJ 08822 CONTACT: REBECCA NEWMAN, UTILITIES CLERK

WATER: (TEL.# 908-782-8840) BOROUGH OF FLEMINGTON WATER DEPT. 38 PARK AVENUE FLEMINGTON, NJ 08822 CONTACT: REBECCA NEWMAN, UTILITIES CLERK

CABLE: (TEL.# 732-652-2775) COMCAST CABLE 100 RANDOLPH ROAD SOMERSET, NJ 08873 CONTACT: SAL DIMAGGIO



KEY MAP 1"=1000'

NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2007 AS AMENDED BY THE SUPPLEMENTARY SPECIFICATIONS FOR FEDERAL AID PROJECTS, EDITION 2011, TO GOVERN UNLESS OTHERWISE INDICATED IN THESE PLANS OR THE PROJECT SPECIFICATIONS AND IN ACCORDANCE WITH CURRENT NJDOT STANDARD CONSTRUCTION DETAILS

APRIL 2017 REVISED THROUGH JUNE 2018

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ENGINEER: Xeller & Kirkpatrick 301 Gibraltar Drive, Suite 2A, Morris Plains, NJ, 07950 973-377-8500 Certificate of Authorization #24GA28037400

John Gi

ANDREW CANGIANO, Professional Engineer, Lic. 43111

PROJ. No. 2016020

GENERAL NOTES:

(ALL NOTES ARE DIRECTED TO THE CONTRACTOR WITHOUT EXCEPTION.)

STANDARDS

- ALL CONSTRUCTION SHALL CONFORM TO THE 2007 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE NEW JERSEY DEPARTMENT OF TRANSPORTATION AND AS AMENDED HEREIN.
- ALL CONSTRUCTION SHALL COMPLY WITH THE LATEST EDITION OF THE LAND DEVELOPMENT ORDINANCE AND STANDARD CONSTRUCTION DETAILS FOR THE BOROUGH OF FLEMINGTON.
- 3. MATERIALS NOT SPECIFIED ON THE PLANS AND USED FOR EXTRA WORK, SHALL CONFORM TO THE 2007 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE DEPARTMENT OF TRANSPORTATION OR THE SUPPLEMENTS, WHICHEVER GOVERNS.
- 4. ONLY AMERICAN MADE PRODUCTS WILL BE ACCEPTED FOR THIS PROJECT. FOREIGN MADE PRODUCTS WILL BE REJECTED DURING SHOP DRAWING REVIEW. FOREIGN MADE PRODUCTS SHIPPED TO THE PROJECT SITE WILL BE REJECTED IN THE FIELD AND RETURNED AT THE CONTRACTORS EXPENSE.

EXISTING CONDITIONS

- 5. EXISTING CONDITIONS TAKEN FROM A MAP ENTITLED "EXISTING CONDITIONS, MAIN STREET, BOROUGH OF FLEMINGTON, HUNTERDON COUNTY. NEW JERSEY", PREPARED BY KELLER & KIRKPATRICK, INC., MORRIS PLAINS, NEW JERSEY, DATED 3/25/16.
- . VERIFY ALL INFORMATION SHOWN OR NOTED FOR EXISTING FACILITIES, GRADES, ROADWAYS, AND MATERIALS. ALL MEASUREMENTS ARE APPROXIMATE. NOTIFY ENGINEER IMMEDIATELY WHEN DISCREPENCIES ARE NOTED IN THE FIELD. NO ADDITIONAL COSTS WILL BE ENCUMBERED BY THE BOROUGH DUE TO THE CONTRACTOR'S FAILURE TO VERIFY INFORMATION PRIOR TO BID SUBMITTAL.
- 7. CONSTRUCTION STAKEOUT WILL BE COMPLETED BY THE CONTRACTOR AND ALL CUT SHEETS VERIFIED BY RESIDENT ENGINEER. THE CONTRACTOR SHALL COORDINATE ALL STAKEOUT REQUIREMENTS AS NOTED IN THE DETAILED SPECIFICATIONS.
- 8. LOCATIONS OF UTILITIES AS SHOWN ARE FROM ABOVE GROUND OBSERVATION AND ARE NOT GUARANTEED. THE CONTRACTOR IS TO CONTACT UTILITY COMPANIES 72 HOURS PRIOR TO CONSTRUCTION TO DETERMINE EXACT LOCATION AND DEPTH AT ALL CROSSINGS IN ACCORDANCE WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL USE UTILITY LOCATIONS AS AN AID IN DETERMINING EXACT LOCATIONS. THE CONTRACTOR SHALL FIELD VERIFY ALL UTILITIES PRIOR TO EXCAVATION.

COORDINATION

- 9. REMOVE EXISTING STREET SIGNS, SIGNS & SIGN POSTS AND PLANTERS WITHIN THE LIMITS OF WORK. COORDINATE WITH THE DIRECTOR OF PUBLIC WORKS FOR THE REMOVAL, STORAGE AND REPLACEMENT OF SAID ITEMS. ALL COSTS ASSOCIATED WITH REMOVAL AND COORDINATION ARE INCLUDED IN THE LUMP SUM PRICE BID FOR "CLEARING SITE"
- 10. NOTIFY ADJACENT PROPERTY OWNERS IN WRITING A MINIMUM OF 5 WORKING DAYS PRIOR TO THE EXECUTION OF WORK TO ALLOW FOR THE REMOVAL OF PERSONAL PROPERTY (LANDSCAPING, SHRUBS, ETC.).

WORK SCHEDULE / MAINTENANCE & PROTECTION OF TRAFFIC

- THE BOROUGH ENGINEER OR HIS DESIGNATED SITE REPRESENTATIVE IS RESPONSIBLE FOR THE IMPLEMENTATION OF THE TRAFFIC CONTROL PLAN. AS THE RESPONSIBLE PARTY, THE BOROUGH ENGINEER OR HIS REPRESENTATIVE SHALL IMMEDIATELY STOP WORK IF/WHEN THE TRAFFIC CONTROL MEASURES AS DETAILED HAVE NOT BEEN IMPLEMENTED.
- 12. NOTIFY ADJACENT PROPERTY OWNERS IN WRITING A MINIMUM OF 5 WORKING DAYS PRIOR TO THE EXECUTION OF WORK TO ALLOW FOR THE REMOVAL OF PERSONAL PROPERTY (LANDSCAPING, SHRUBS, ETC.).
- 13. COMPLY WITH THE LATEST EDITION OF THE 'MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" WHEN UTILIZING ALL DEVICES AND PROCEDURES FOR THE MAINTENANCE AND PROTECTION OF TRAFFIC. EXECUTE WORK TO ENSURE THE CONVENIENT AND SAFE PASSAGE OF ALL VEHICULAR AND PEDESTRIAN TRAFFIC ON ADJACENT STREETS.
- 4. FLAGGERS SHALL BE UTILIZED ON WITHIN THE PROJECT LIMITS DURING CONSTRUCTION IN ORDER TO SAFE WORKING CONDITIONS AT ALL TIMES. UNIFORMED TRAFFIC DIRECTORS SHALL BE UTILIZED AS APPROVED BY THE POLICE CHIEF, ENGINEER AND PUBLIC WORKS DIRECTOR. UNIFORMED TRAFFIC DIRECTORS SHALL NOT BE UTILIZED AS FLAGGERS.
- 15. COORDINATE THE LOCATION OF ALL TRAFFIC CONTROL DEVICES (SIGNS, CONES, BARRICADES AND DRUMS) WITH THETHE BOROUGH ENGINEER OR HIS DESIGNATED SITE REPRESENTATIVE, THE BOROUGH ENGINEER OR HIS DESIGNATED SITE REPRESENTATIVE MUST APPROVE ALL TRAFFIC CONTROL MEASURES UTILIZED ON THE PROJECT SITE. STOP WORK ORDERS WILL BE ISSUED IF THE TRAFFIC CONTROL MEASURES TAKEN ARE DEEMED INSUFFICIENT BY THE BOROUGH POLICE DEPARTMENT. ALL COSTS ASSOCIATED WITH THE PROVISION OF TRAFFIC CONTROL MEASURES IS INCLUDED IN THE UNIT PRICES BID FOR "CONSTRUCTION SIGNS", "TRAFFIC CONES", "BREAKAWAY BARRICADES, TYPE III" AND "TRAFFIC DRUMS". NO SEPARATE PAYMENTS WILL BE MADE FOR RELOCATING THE DEVICES AS REQUIRED, OR AS DIRECTED BY THE BOROUGH ENGINEER OR HIS DESIGNATED SITE REPRESENTATIVE DURING THE COURSE OF CONSTRUCTION.
- 16. THE CONTRACTOR MUST COORDINATE ALL WORK ON DRIVEWAYS WITH THE INDIVIDUAL PROPERTY OWNERS PRIOR TO START OF CONSTRUCTION THE BOROUGH OF FLEMINGTON IS NOT RESPONSIBLE FOR DELAYS CAUSED BY FAILURE OF THE CONTRACTOR TO COORDINATE WITH THE INDIVIDUAL PROPERTY OWNERS. NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK. ALL COSTS ASSOCIATED WITH THE SUSPENSION OF PARKING AND COORDINATION WITH THE PROPERTY OWNERS MUST BE INCLUDED. IN THE VARIOUS PAY ITEMS PROVIDED IN THE CONTRACT.
- 17. EXISTING TRAFFIC CONTROL SIGNS IMPACTED BY THE PROPOSED WORK SHALL BE REMOVED, PROTECTED, AND REINSTALLED IN ACCORDANCE WITH MUTCO STANDARDS, INCLUDING BREAKAWAY HA

SOIL EROSION AND SEDIMENT CONTROL

- 18. NO SEPARATE PAYMENT WILL BE MADE FOR SOIL EROSION & SEDIMENT CONTROL. ALL COSTS ASSOCIATED WITH THIS WORK IS INCLUDED IN THE VARIOUS PAY ITEMS PROVIDED IN THE CONTRACT.
- 19. ROADWAY MUST BE SWEPT CLEAN AT THE END OF EACH WORK DAY AND AS DIRECTED BY THE BOROUGH ENGINEER OR HIS FIELD REPRESENTATIVE. DUST CONTROL WILL BE REQUIRED AT ALL TIMES AS DETERMINED BY THE ENGINEER. TIRES MUST BE CLEAN ON ALL VEHICLES BEFORE ENTERING ROADWAY.

DEMOLITION

- 20. DISPOSE OF ALL MATERIALS EXCAVATED REGARDLESS OF THEIR NATURE. THE BOROUGH IS NOT OBLIGATED TO SUPPLY A DISPOSAL SITE. DO NOT DEPOSIT ANY EXCESS MATERIALS WITHIN THE BOROUGH LIMITS WITHOUT WRITTEN PERMISSION OF THE BOROUGH ENGINEER. MATERIALS MUST BE DISPOSED OF IN ACCORDANCE WITH ALL STATE REGULATIONS REGARDING SAME. ALL COSTS ASSOCIATED WITH THIS WORK IS INCLUDED IN THE UNIT PRICE BID FOR "CLEARING SITE".
- LOAD ALL MATERIALS / DEBRIS GENERATED DURING THE DEMOLITION OF CURB AND SIDEWALK INTO A DUMP TRUCK AND HAUL OFFSITE IMMEDIATELY. DO NOT STOCKPILE DEMOLITION MATERIALS WITHIN THE PROJECT LIMITS. ALL COSTS ASSOCIATED WITH THIS WORK IS INCLUDED IN THE UNIT PRICE BID FOR "CLEARING SITE".
- 22. LOAD ALL MATERIALS/ DEBRIS GENERATED DURING THE REMOVAL OF EXISTING TREES, SHRUBS AND STUMPS INTO A DUMP TRUCK AND HAUL OFFSITE IMMEDIATELY, DO NOT STOCKPILE DEMOLITION MATERIALS WITHIN THE PROJECT LIMITS. ALL COSTS ASSOCIATED WITH THIS WORK IS INCLUDED IN THE UNIT PRICE BID FOR "CLEARING SITE".
- 23. EXISTING CURB CONSISTS OF BOTH CONCRETE AND BELGIAN BLOCK, DIMENSIONS VARY. THE CONTRACT CALLS FOR REMOVAL AND REPLACEMENT OF EXISITING CURB. THE ACTUAL LIMITS OF REMOVAL AND REPLACEMENT MUST BE VERIFIED BY THE BOROUGH ENGINEER PRIOR TO WORK. THE BOROUGH OF FLEMINGTON IS NOT RESPONSIBLE FOR ANY CURB REMOVED PRIOR TO VERIFICATION, REPLACEMENT OF SAID CURB WILL BE AT THE CONTRACTOR'S EXPENSE. ALL COSTS ASSOCIATED WITH THIS WORK IS INCLUDED IN THE UNIT PRICE BID FOR "CLEARING SITE".
- 24. SAWCUT THE EXISTING ASPHALT DRIVEWAYS ADJACENT TO DRIVEWAY APRONS SCHEDULED FOR REMOVAL AND REPLACEMENT. ALL COSTS ASSOCIATED WITH THIS WORK IS INCLUDED IN THE UNIT PRICE BID FOR "CLEARING SITE".

UTILITIES

- 25. PROTECT ALL EXISTING UTILITIES AND COORDINATE THE RELOCATION OF ALL CONFLICTING UTILITIES WITH THE RESPECTIVE UTILITY COMPANY.
- 26. MINIMIZE THE NUMBER AND FREQUENCY OF OPEN TRENCHES OR EXCAVATIONS (ADJACENT TO NEW CURB INSTALLATION). COVER ALL TRENCHES OR EXCAVATIONS AT THE CONCLUSION OF WORK EACH SHIFT. PLACE COMPACTED DENSE GRADED AGGREGATE IN OPEN TRENCHES OR EXCAVATIONS AT THE END OF EACH SHIFT.
 - AVAILABLE INFORMATION AS TO THE LOCATION OF EXISTING UTILITIES HAS BEEN COLLECTED FROM VARIOUS SOURCES. THE RESULTS OF SUCH INVESTIGATIONS, REPRESENTED ON THE CONTRACT DRAWINGS ARE NOT GUARANTEED AS TO THE ACCURACY. ALL EXISTING UTILITIES ARE SHOWN FOR INFORMATION ONLY. CONTACT THE GARDEN STATE UNDERGROUND PLANT LOCATION SERVICE INC AT PHONE # 1-800-272-1000 A MINIMUM OF ONE WEEK (5 WORKING DAYS) PRIOR TO COMMENCEMENT OF WORK. LOCATE, IDENTIFY AND PROTECT UTILITIES WITHIN THE PROJECT LIMITS. NO SEPARATE PAYMENT WILL BE PROVIDED FOR THE UTILIZATION OF TEST PITS TO LOCATE UTILITIES. ALL COST ASSOCIATED WITH PERFORMING TEST PITS IS INCLUDED IN THE VARIOUS PAY ITEMS.
 - B. PROTECT UNDERGROUND FACILITIES OR UTILITY POLES WITHIN CLOSE PROXIMITY OF EXCAVATIONS OR TRENCHING OPERATIONS TO PREVENT DAMAGE OR INTERRUPTION OF SERVICE TO UNDERGROUND FACILITIES.
 - C. REPAIR ANY DAMAGED UTILITIES AT NO ADDITIONAL COST TO THE OWNER.
 - D. CONTACT PRIVATE UTILITY COMPANIES SO THAT PRIVATELY OWNED CASTINGS MAY BE RESET BY RESPECTIVE UTILITY COMPANY TO PROPOSED GRADES.
- 27. INSTALL ALL STRUCTURES IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND DETAILS.
- 28. COORDINATE ANY UTILITY ADJUSTMENTS NECESSITATED BY THE PROPOSED CONSTRUCTION WITH THE BOROUGH AND EACH OF THE UTILITY COMPANIES WITH FACILITIES WITHIN THE PROJECT LIMITS. NOTIFY ALL UTILITY COMPANIES AT LEAST ONE (1) WEEK PRIOR TO CONSTRUCTION FOR PHYSICAL MARK OUTS OF UTILITIES.

BID ITEMS

- CONSTRUCTION DETAILS.
- THICKNESS) AND 9.5 M-64 (2" COMPACTED THICKNESS).
- CONSTRUCTION DETAILS.

RESTORATION

- VARIOUS PAY ITEMS.
- THE VARIOUS PAY ITEMS.
- INCLUDED IN THE UNIT PRICE BID FOR "CLEARING SITE".
- GUARANTEE PERIOD.

ITEM	DESCRIPTION	UNIT	QUANTITY	QUANTITY SHEET 8	QUANTITY SHEET 9	QUANTITY SHEET 10	QUANTITY SHEET 11	QUANTITY SHEET 15	QUANTITY SHEET 16		QUANTITY SHEET 18	QUANTITY SHEET 19	QUANTITY SHEET 20
		LS	1							-			
	MOBILIZATION	LS	1									<u> </u>	
3	CONSTRUCTION LAYOUT	LS	1										
4	SOIL EROSION & SEDIMENT CONTROL MEASURES	LS	1										
5	BREAKAWAY BARRICADE	UNITS	60										
6	DRUM	UNITS	30										
7	TRAFFIC CONE	UNITS	70										
8	CONSTRUCTION SIGNS	SF	300										
9	FLASHING ARROW BOARD, 2' X 4'	UNIT	1										
10	PORTABLE VARIABLE MESSAGE SIGN	UNIT	1										
11	TRAFFIC DIRECTOR, FLAGGER	МН	250										
12	POLICE TRAFFIC DIRECTORS (ALLOWANCE)	МН	250										
13	EXCAVATION, TEST PIT	CY	10										
14	TRENCH DRAIN	LF				20	55						
15	12" DUCTILE IRON PIPE, CLASS 52	LF					28						
16	INLET, TYPE B	UNIT					1						
17	RECONSTRUCT INLET, TYPE E, USING NEW CASTING COMPLETE	UNIT					2						
	RECONSTRUCTED INLET, TYPE B, USING NEW CASTING COMPLETE	UNITS					4						
19	CONCRETE SIDEWALK, 4" THICK	SY		1540	987								
20	CONCRETE SIDEWALK, 4" THICK TINTED	SY			45								
21	CONCRETE DRIVEWAY, REINFORCED, 6" THICK	SY		68	155								
22	HOT MIX ASPHALT DRIVEWAY, 6" THICK	SY		19	68						•		
23	DETECTABLE WARNING SURFACE, CAST IRON	SY		6.3	8.1								
24	9"x18" CONCRET E VERTICAL CURB	LF		1092	1108								
25	TRAFFIC MARKINGS, THERMOPLASTIC	SF										891	819
26	REMOVAL OF TRAFFIC STRIPES	LS	1										
27	RELOCATE/RESET SIGN WITH NEW SIGN & BREAKAWAY U-POST	UNITS										22	19
28	STREET SIGN ON ANTIQUE POST	UNITS										2	2
29	TEMPORARY RECESSED SIGN SUPPORT BASE	UNITS		18	10								
30	TRASH RECEPTACLE, DECORATIVE	UNITS	8										
31	TRASH STATION, DECORATIVE	UNI⊺S	6										
32	BENCHES - (6')	UNITS		18	3								
33	BIKE RACK	UNITS		2	2								
34	HANDHOLE/SPLICE BOX	UNITS						17	16				
35	1.5" RIGID NONMETALLIC CONDUIT, SCHEDULE 80 PVC	LF						719	970				
36	2.0" RIGID NONMETALLIC CONDUIT, SCHEDULE 80 PVC	LF						20	20				
37	FOUNDATION TYPE OLF	UNIT						17	16				
38	FOUNDATION TYPE MC	UNIT						1	1				
39	GROUND WIRE, NO.8 AWG	LF						1668	1780				
	SERVICE WIRE, NO. 2 AWG	LF						60	60				
	LIGHTING WIRE, NO. 8 AWG	LF						4170	4450				
42	METER CABINET, TYPE TL, (POWDER COATED)	UNITS						1	1				
43	LIGHT, STANDARD ALUMINUM	UNITS	3					17	16				
44	LUMINAIRE	UNITS	3					17	16				
45	TREE PITS (INCLUDES TOPSOIL & PIT GUARD)	UNITS		21	8								
	SMALL DECIDUOUS TREE, 2-2 1/2" CALIPER	UNITS								21	8		
	6" SCH 40 PVC STORM SEWER LATERAL	LF					15						
	8" SCH 40 PVC UNDERDRAIN TYPE F	LF					150						
49	EPOXY WAT ERPROOFING	SY		25									



THE LUMP SUM PRICE BID FOR "CLEARING SITE" INCLUDES ALL COSTS ASSOCIATED WITH LABOR, EQUIPMENT AND MATERIALS REQUIRED FOR THE REMOVAL OF ALL VEGETATIVE COVER, SIDEWALK, SIGNS, PLANTERS, DEBRIS, RUBBLE, PAVING OF ANY KIND, DRAINAGE STRUCTURES, EXISTING FEATURES WITHIN THE LIMITS OF WORK WHICH INTERFERE WITH THE EXECUTION OF WORK AS SPECIFIED WITHIN THE CONSTRUCTION DOCUMENTS AND GRUBBING OF TURF AREAS WITHIN PROJECT AREA. ALL WORK MUST BE EXECUTED IN ACCORDANCE WITH NJDOT SPECIFICATIONS.

30. THE UNIT PRICES BID FOR "EXCAVATION, TEST PIT" AND "EXCAVATION, UNCLASSIFIED" INCLUDES ALL LABOR, EQUIPMENT AND MATERIAL COSTS ASSOCIATED WITH THE EXCAVATION, AND REMOVAL OF ALL MATERIAL TO THE ROADWAY AREAS AS NOTED ON THE PLANS, AND SPECIFICATIONS. THE CONTRACTOR SHALL EXCAVATE AND REMOVE THE MATERIAL AS NOTED IN THE SUPPLEMENTAL SPECIFICATIONS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR EMBANKMENT FILL UTILIZING EXISTING MATERIAL AND OTHER ITEMS NOTED IN THE SPECIFICATIONS. THE CONTRACTOR SHALL EXCAVATE TEST PITS NOTED ON THE PLANS AND AS DIRECTED PRIOR TO THE INSTALLATION OF STORM SEWER PIPE. THE ENGINEER OR HIS AUTHORIZED REPRESENTATIVE SHALL BE PRESENT DURING THE EXCAVATION OF TEST PITS.

31. THE UNIT PRICE BID FOR "DENSE GRADED AGGREGATE BASE COURSE VARIABLE THICKNESS" SHALL INCLUDE UNCLASSIFIED EXCAVATION, REMOVAL AND PROPER DISPOSAL OF ALL UNSUITABLE SUBBASE MATERIAL, PLACEMENT AND COMPACTION OF THE BASE COURSE TO THE SATISFACTION OF THE ENGINEER, AND ALL OTHER INCEDENTIAL ITEMS.

32. THE UNIT PRICE BID FOR "ALL REINFORCED CONCRETE PIPE ITEMS" AND "INLET ITEMS" SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIAL FOR UNCLASSIFIED EXCAVATION, REMOVAL AND PROPER DISPOSAL OF EXCAVATED TRENCH MATERIAL, PIPE PLACEMENT, BEDDING AND DGABC BACKFILL AS NOTED ON THE CONSTRUCTION DETAILS.

33. THE UNIT PRICE BID FOR CURB AND SIDEWALK ITEMS INCLUDES ALL LABOR, EQUIPMENT AND MATERIAL COSTS ASSOCIATED WITH THE UNCLASSIFIED EXCAVATION FOR CURB AND SIDEWALK, SAWCUTTING AND REMOVAL AND DISPOSAL OF ALL EXISTING CONCRETE, STONE BEDDING AND REINFORCEMENT (AS NOTED ON THE DETAILS), AND CONCRETE AS NOTED IN THE

34. THE UNIT PRICE BID FOR "HOT MIX ASPHALT DRIVEWAY, VARIABLE THICKNESS" INCLUDES ALL LABOR, EQUIPMENT, AND MATERIAL IN ORDER TO COMPLETE SAWCUTTING OF THE EXISTING DRIVEWAY, UNCLASSIFIED EXCAVATION, REMOVAL AND PROPER DISPOSAL OF EXCAVATED MATERIAL, FINE GRADING, COMPACTION, AND INSTALLATION OF 25M-64 (3" COMPACTED

35. THE UNIT PRICES BID FOR "LAWN RESTORATION ITEMS" INCLUDES ALL LABOR, EQUIPMENT AND MATERIAL COSTS ASSOCIATED WITH INSTALLATION OF TOPSOIL, FERTILIZER, SEED, MULCH, AND EROSION CONTROL MATTING AS SHOWN ON THE PLANS OR AS DIRECTED IN ACCORDANCE WITH THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT NOTES AS NOTED IN THE

36. SUBMIT PRECONSTRUCTION VIDEOS AND PHOTOGRAPHS OF THE PROJECT SITE PRIOR TO COMMENCEMENT OF WORK TO SERVE AS EVIDENCE OF EXISTING CONDITIONS IN CASE OF FUTURE DISPUTE WITH ADJACENT PROPERTY OWNERS. WITHOUT ADEQUATE DOCUMENTATION TO DISPUTE AN ADJACENT PROPERTY OWNERS CLAIM, CONTRACTOR WILL BE FINANCIALLY RESPONSIBLE FOR ALL LABOR, EQUIPMENT AND MATERIALS REQUIRED TO REESTABLISH EXISTING CONDITIONS. NO SEPERATE PAYMENT WILL B MADE FOR THE PREPARATION OF VIDEOS AND PHOTOGRAPHS. ALL COSTS ASSOCIATED WITH THIS WORK IS INCLUDED IN THE

37. RESTORE ALL DISTURBED SHRUBS, TREES, LAWNS, FENCING, SIGNS, LIGHT POLES, DEVICES, PAVING, ETC. TO THEIR ORIGINAL CONDITION, AND TO THE SATISFACTION OF THE OWNER. NO SEPARATE PAYMENT SHALL BE MADE FOR THIS RESTORATION. ALL LABOR, EQUIPMENT AND MATERIALS REQUIRED TO COMPLETE THIS WORK WIL BE INCLUDED IN THE UNIT PRICES BID FOR

38. THE REMOVAL OF LIMBS AND BRANCHES DAMAGED DURING CONSTRUCTION OR DEAD BRANCHES AND TRIMMING OF LIMBS FROM EXISTING TREES SCHEDULED TO REMAIN SHALL BE PERFORMED UNDER THE DIRECT SUPERVISION OF A CERTIFIED LANDSCAPE ARCHITECT OR TREE SPECIALIST. THE COSTS ASSOCIATED WITH THE LABOR, EQUIPMENT AND MATERIALS IS

39. NEW TREES SHALL BE OF HIGH QUALITY NURSERY STOCK. CONTRACTOR SHALL SUBMIT NAME OF THE NURSERY FOR APPROVAL PRIOR TO INSTALLATION OF TREES. UNIT PRICE SHALL INCLUDE FURNISH AND INSTALLATION OF TREES PER THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR WATERING TREES AND MAINTENANCE DURING CONSTRUCTION AND THE

ADDITIONAL NOTES

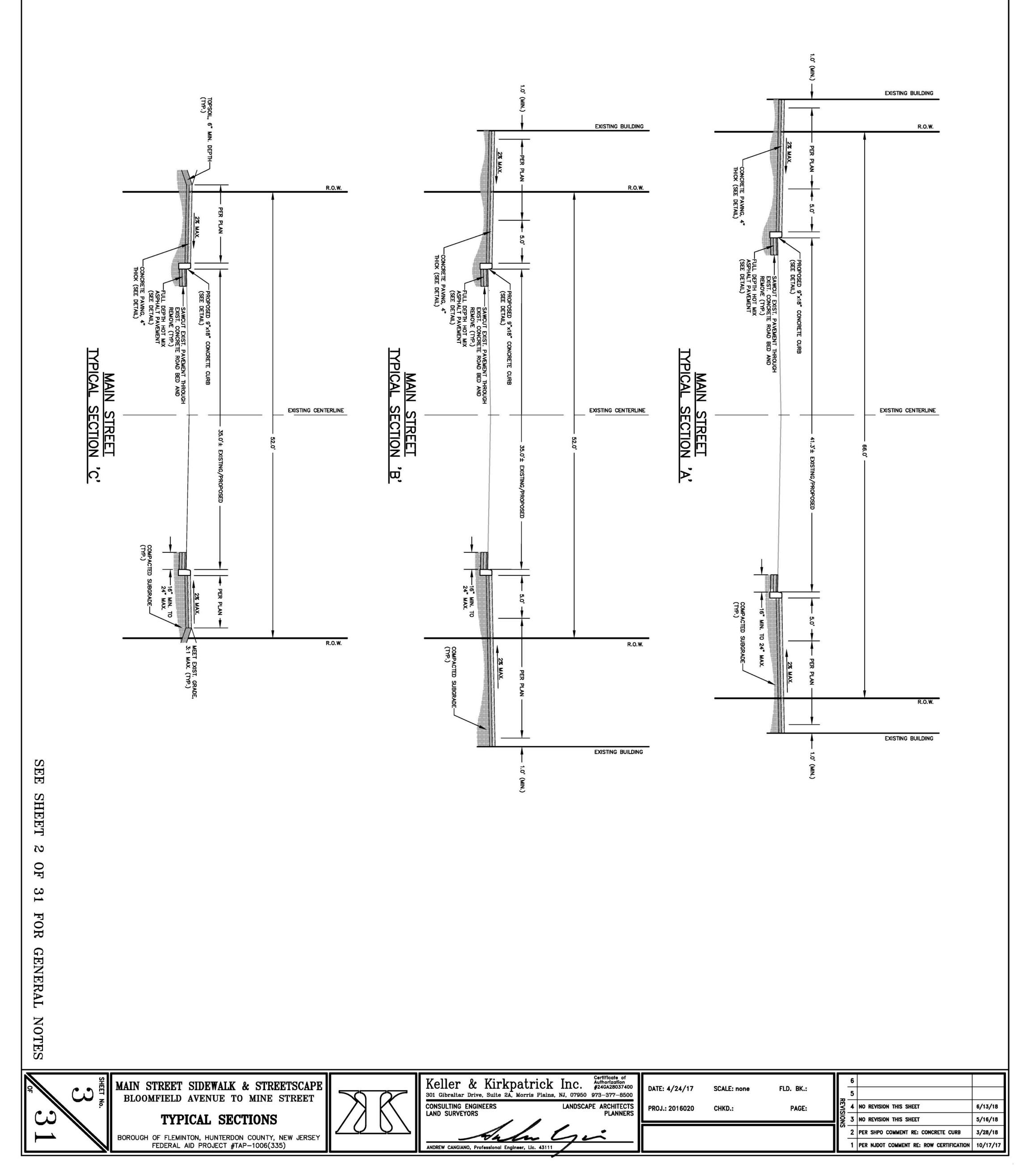
- 1. THE CONTRACTOR SHALL VISIT THE SITE AND FAMILIARIZE HIMSELF WITH THE EXISTING CONDITIONS PRIOR TO SUBMITTING HIS BID. ANY DISCREPANCIES BETWEEN THESE DRAWINGS AND THE ACTUAL CONDITIONS AT THE SITE SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.
- 2. THE CONTRACTOR SHALL CARRY OUT HIS WORK CAREFULLY AND SKILLFULLY TO AVOID ANY DISTURBANCE OF UTILITY LINES, VALVES, POLES, PUBLIC OR PRIVATE PROPERTY, REPAIR ANY EXISTING LANDSCAPE FEATURES AND MONUMENTS. THE CONTRACTOR SHA PROMPTLY REPAIR DAMAGE DONE TO THESE ABOVE MENTIONED ITEMS WITHOUT COST THE STATE, COUNTY OR TOWNSHIP AND IN A MANNER SATISFACTORY TO THE ENGINEER.
- 3. ALL TRENCHES EXPOSED DURING REMOVAL OF EXISTING PIPES, INLETS, AND/OR MANHOLES ARE TO BE BACKFILLED WITH NEW MATERIALS.
- 4. PROVIDE A SMOOTH SAWCUT EDGE IN ALL CASES WHERE PROPOSED PAVEMENT OR CURBS ABUTS EXISTING PAVEMENTS OR CURBS. NO SEPARATE PAYMENT FOR SAWCUT.
- 5. CONTRACTOR SHALL BE ADVISED THAT EXISTING SIDEWALKS AND PAVEMENT MAY CONTAIN EXISTING CONCRETE BASE AND SHALL MAKE ALL NECESSARY REQUIREMENTS FOR ITS REMOVAL AT NO ADDITIONAL COST.
- 6. SUBSTITUTIONS OF PROPRIETARY MATERIAL AND/OR PRODUCT SPECIFICATIONS THAT NOTE "OR EQUAL" MUST BE APPROVED BY THE BOROUGH ENGINEER.
- 7. THE CONTRACTOR MUST VERIFY ALL DIMENSIONS AND GRADES. IF ANY CONFLICTS, ERRORS, AMBIGUITIES OR DISCREPANCIES ARE FOUND IN THE CONSTRUCTION DRAWINGS OR SPECIFICATIONS, THE CONTRACTOR IS TO NOTIFY THE TOWNSHIP ENGINE
- B. THE CONTRACTOR SHALL PROTECT ALL LANDSCAPING DURING CONSTRUCTION UNTIL ACCEPTANCE OCCURS. ANY DAMAGE DONE TO EXISTING VEGETATION DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT NO EXPENSE TO THE C
- 9. THE CONTRACTOR SHALL OBTAIN APPROVAL OF THE TREES & SHRUBS FROM THE TON ENGINEER BEFORE INSTALLATION.

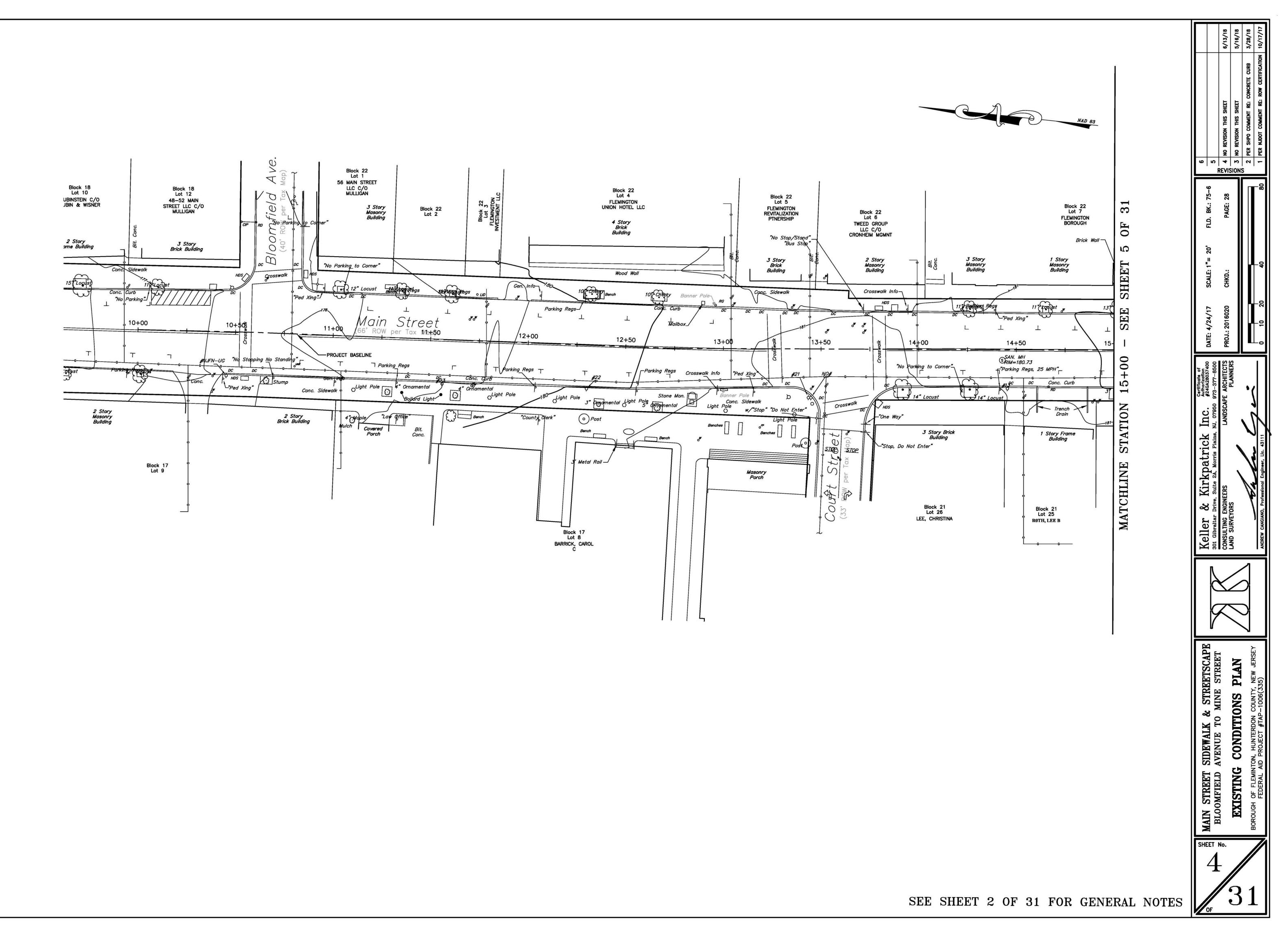
ADDITIONAL NJDOT NOTES

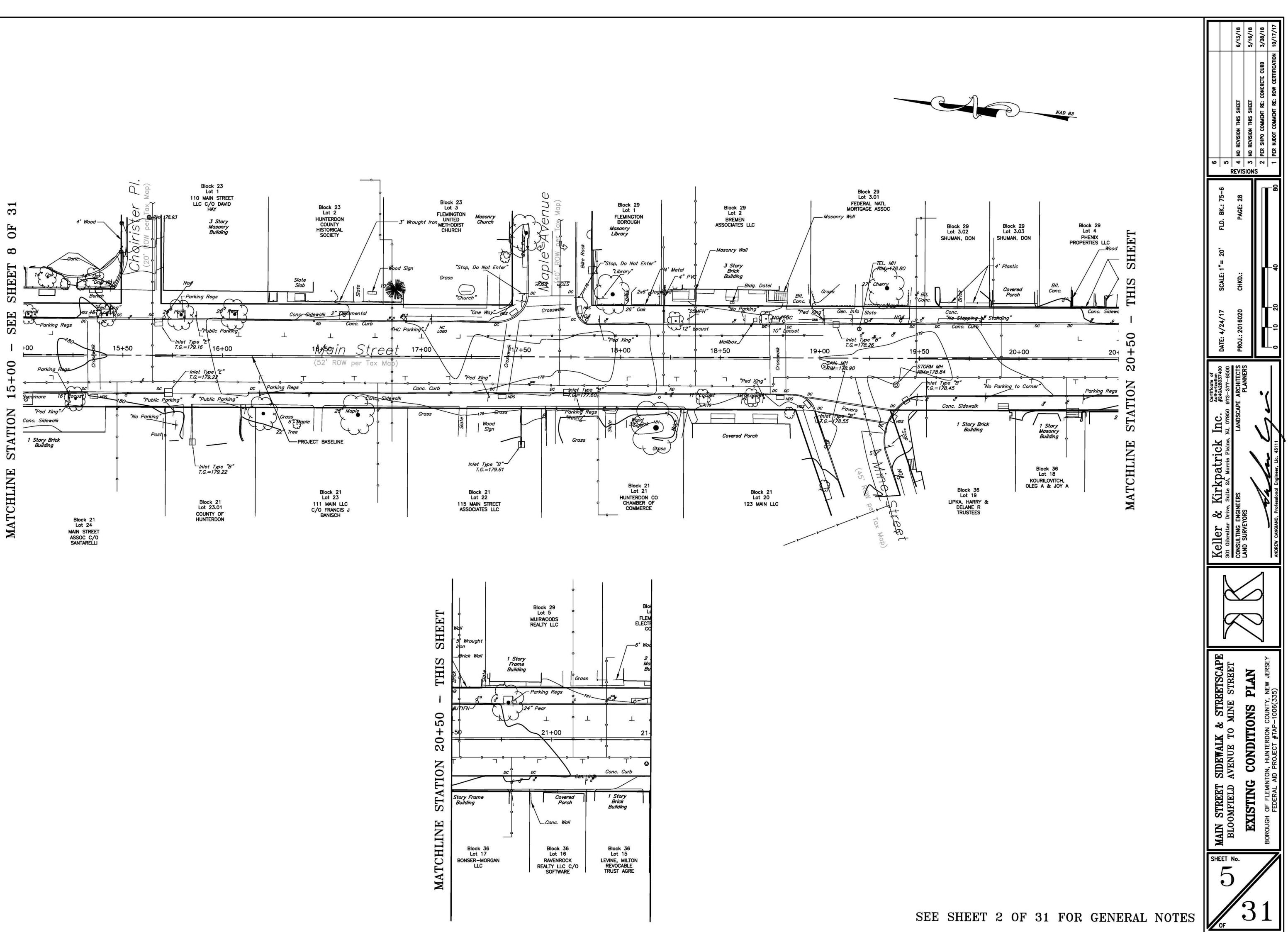
- 1. BEST MANAGEMENT PRACTICES WILL BE FOLLOWED FOR THE CONSTRUCTION OF THIS
- 2. CONSTRUCTION STAGING ACTIVITIES (INCLUDING THE STORAGE OF EQUIPMENT/VEHICI ARE PROHIBITED IN ENVIRONMENTALLY SENSITIVE AREAS. IF THE CONTRACTOR WISH THESE AREAS OR ACCESS THESE AREAS FOR ANY REASON, ALL APPROPRIATE PER APPROVALS SHALL BE OBTAINED PRIOR TO SUCH USE.
- 3. IF THE PROPOSED ACTIVITIES INCLUDE DIRECT CONTACT WITH INLETS, THEY MUST B TO MEET THE NEW JERSEY POLLUTANT DISCHARGE ELIMINATION SYSTEM (NJPDES) S REQUIREMENTS.
- 4. IF THERE ARE ANY CHANGES TO THE PROPOSED ACTIVITIES, THE CHANGES MUST B BY THE BEPR TO DETERMINE THE NEED FOR ADDITIONAL REGULATORY COMPLIANCE.

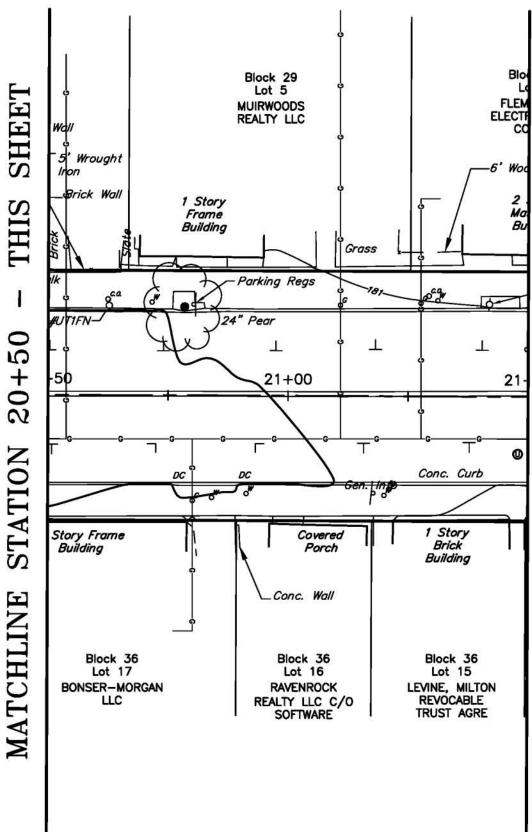
ESTIMATE OF QUANTITIES

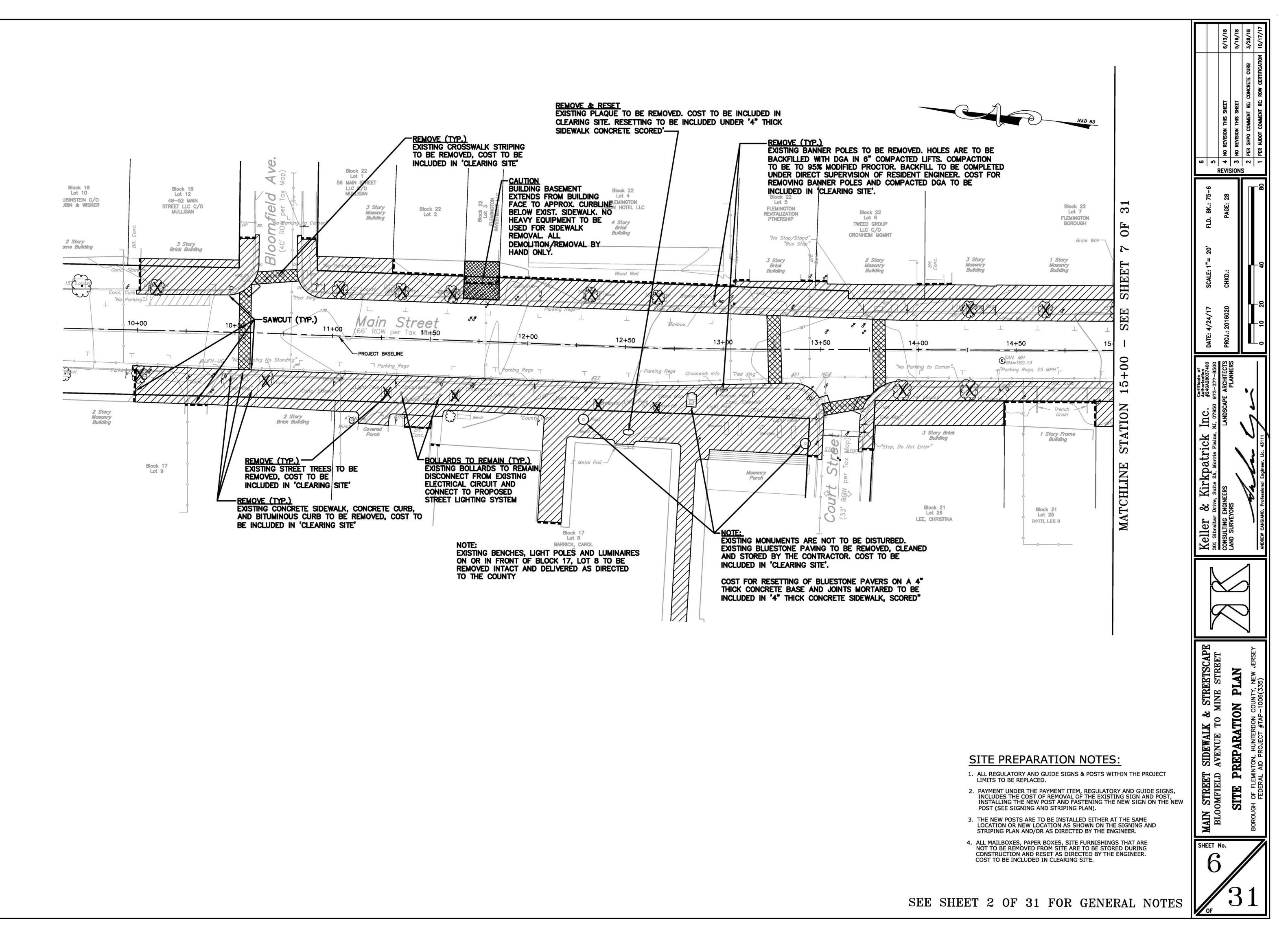
		6/13/18 5/16/18 3/28/18
	10. CONTRACTOR SHALL COORDINATE WITH JCP&L AS TO CONDUIT LOCATIONS FOR ELECTRICAL	CURB CURB
	SERVICES TO STREET LIGHTS TO ASSURE NO CONFLICT WITH OVERHEAD WIRES. NO SEPARATE PAYMENT. 11. CONTRACTOR SHALL BUILD THE LIGHT POLE FOUNDATIONS AND CONDUITS AS SHOWN ON	RE: RE:
	THE PLANS AND PROPERLY PROTECT THEM UNTIL THE LIGHTS ARRIVE. NO SEPARATE PAYMENT.	BEPR COMMENT I THIS SHEET COMMENT RE: C
то	12. CONTRACTOR SHALL BE AWARE THAT THERE ARE VARYING ELEVATIONS AT THE BUSINESS ENTRANCES. FINISHED SURFACES SHALL BE WARPED TO ACCOMMODATE THESE VARYING ELEVATIONS.	NJDOT BEPR COL REVISION THIS SH SHPO COMMENT NJDOT COMMENT
	13. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ALL THE REGULATORY SIGNS AND SIGN POSTS WITHIN THE PROJECT LIMITS. COST TO BE INCLUDED IN THE BID ITEM: "RELOCATE/ RESET SIGN WITH SIGN & BREAKAWAY U-POST".	PER NJI PER SHI
	14. THE CONTRACTOR IS RESPONSIBLE FOR THE CONSTRUCTION LAYOUT. 15. RIGHT-OF-WAY AND LOT LINE INFORMATION TAKEN FROM CURRENT	တ္ တြ န ကြ ကြ ကြ REVISIONS
	TAX MAPS OF THE BOROUGH OF FLEMINGTON 16. HORIZONTAL DATUM IS BASED UPON NAD 83.	
	 VERTICAL DATUM IS BASED UPON NAVD 88. 18. LOT AND BLOCK NUMBERS SHOWN HEREON ARE BASED UPON THE CURRENT TAX RECORDS OF THE BOROUGH OF FLEMINGTON 	- BK.: PAGE:
ER.	19. EXISTING CONDITIONS TAKEN FROM A MAP ENTITLED "EXISTING CONDITIONS, MAIN STREET, BOROUGH OF FLEMINGTON, HUNTERDON	LD.
WNER.	COUNTY, NEW JERSEY", PREPARED BY KELLER & KIRKPATRICK, INC., MORRIS PLAINS, NEW JERSEY, DATED 3/25/16.	ω
M NSHIP		SCALE: NON CHKD.:
		SCALE: CHKD.:
S PROJECT. LES/MATERIALS) ES TO USE		/17 020
MITS AND		DATE: 4/24/17 PROJ.: 2016020
E RETROFITTED		DATE PROJ
e reevaluated		of 37400 8500 ECTS NERS
		Certificate of Authorization 4246a28037400 #246a28037400 973-377-8500 F ARCHITECTS PLANNERS
		о́а́ • • 6 ш
		Inc. NI. 07950 LANDSCI
(] QUANTITY]	STANDARD LEGEND	atrick Morris Plains,
TOTAL	Exist. Manholes (Typ.) W Kater Main	kpatr 2.A. Morris Engineer, Lic.
	O G Exist. Gas Main O T T Exist. Telephone Conduit O T T Exist. Telephone Conduit	
1 60	• E E E E E E E E E Exist. Electric Conduit	
30 70	(Over 30 [®] Draw To Size) (Size And Type) RESET M.H. (Over 30 [®] Draw To Size) (Size And Type) And Storm Drains	C I ING I ING I
300 1	PROPOSED M.H. PROPOSED SANITARY SEWERS AND STORM DRAINS	Keller & 301 Gibraltar Driv consulting engi Land surveyors
1 250 250	Concrete CONCRETE Bituminous HOT MIX ASPHALT PAVEMENTS Exist PROPOSED Exist PROPOSED	
10 75		
28	RIGHT OF WAY - ACCESS PERMITTED PROPERTY LINES	
2 4	Exist PROPOSED CURBS	
2527 45		
223 87	PROPOSED RESET Exist. Inlet B RECONSTRUCT	EET EET Jersey
14.4 2200 1710	CASTING Exist. Inlet E INLETS (B or E)	
1 41	FRAME, CIRCULAR COVER FOR EXISTING INLET NOTE: CURB PIECES FOR INLETS TYPE 'B' ARE 6" UNLESS OTHERWISE INDICATED	K STREETS MINE STRI EGEND , ANTITIES
4 28	Ow Exist. Water Gate Valve 💽 Exist. Deciduous Tree (Size, Kind)	II ~ ~ ~ EE ~ ~ []
8 6	OG Exist. Gas Gate Valve ♀ ♀ Exist. Bush _o_ Exist. Pole No. & Type (Guy, Light, Etc.) ★ Exist. Evergreen	
21 4	∀ Exist. Hydrant CCCCC Exist. Hedges ↓ Exist. Traffic Light Exist. Sign Evist. Traffic Control Box	
33 1689	Exist. Traffic Control Box	
40		
2 3448		1 9 55 63 5
120 8620		MAIN BLO GF J BOROUI
2 36 36		SHEET No.
29 29		2 //
15 150		
25		

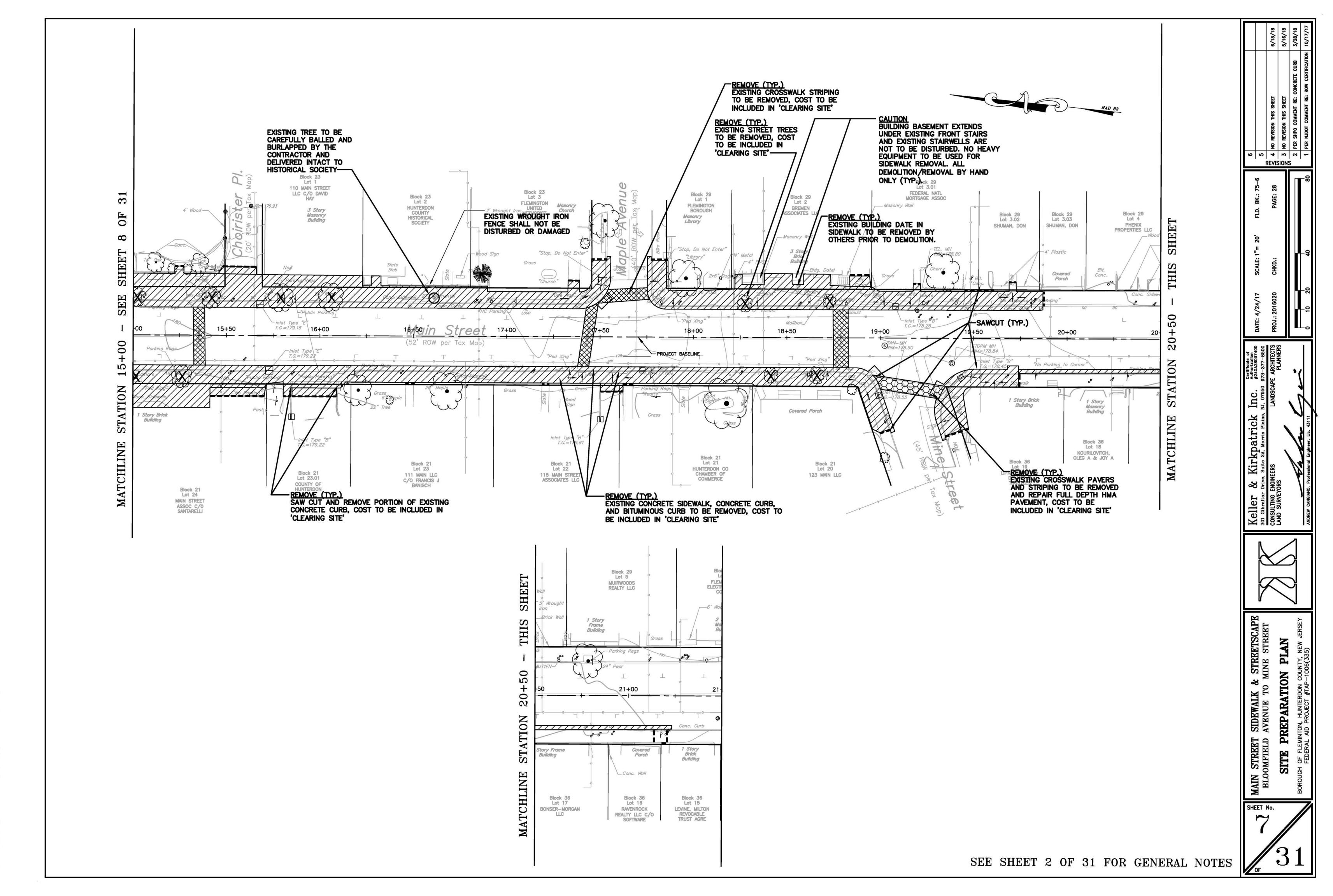


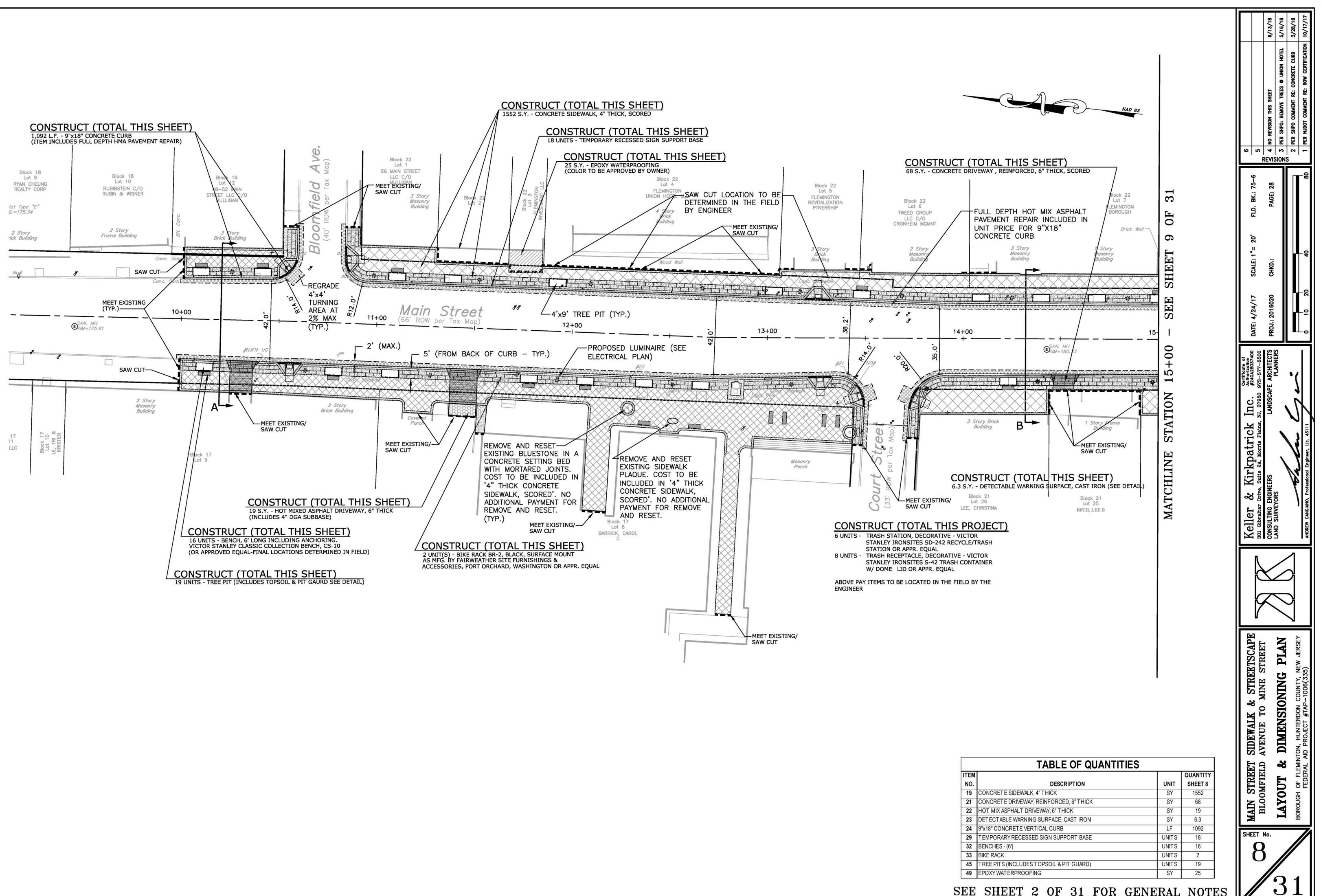






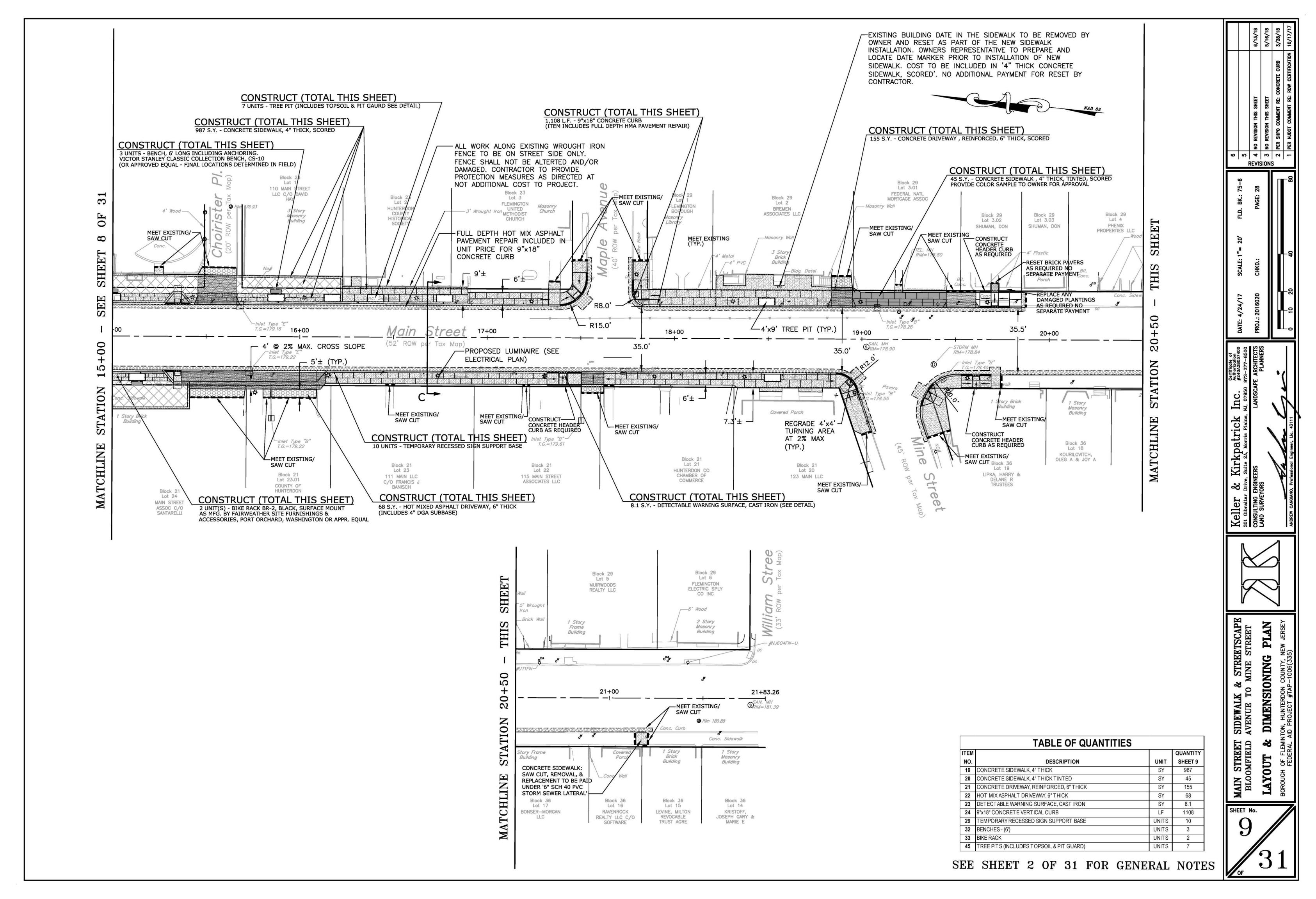


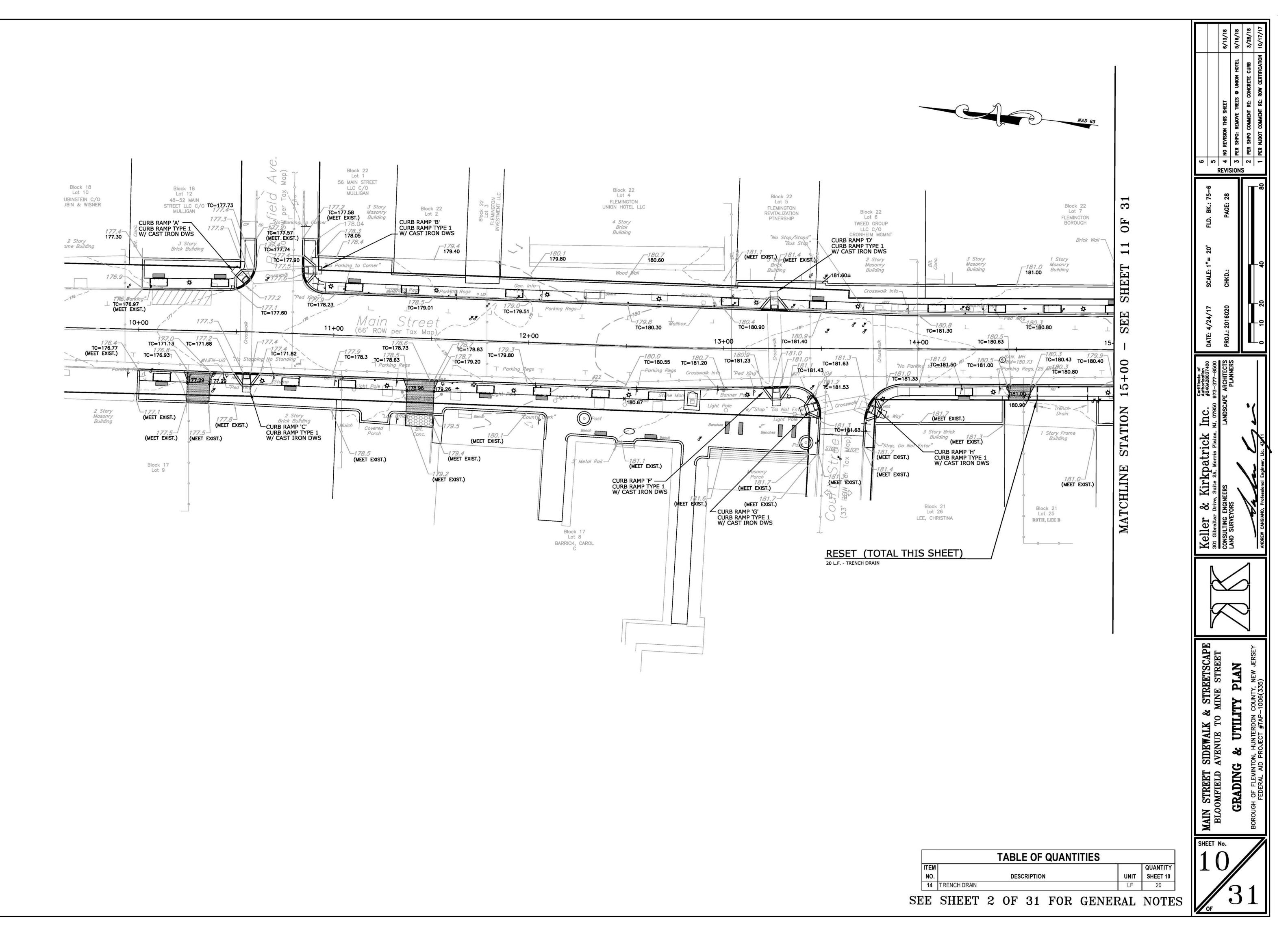


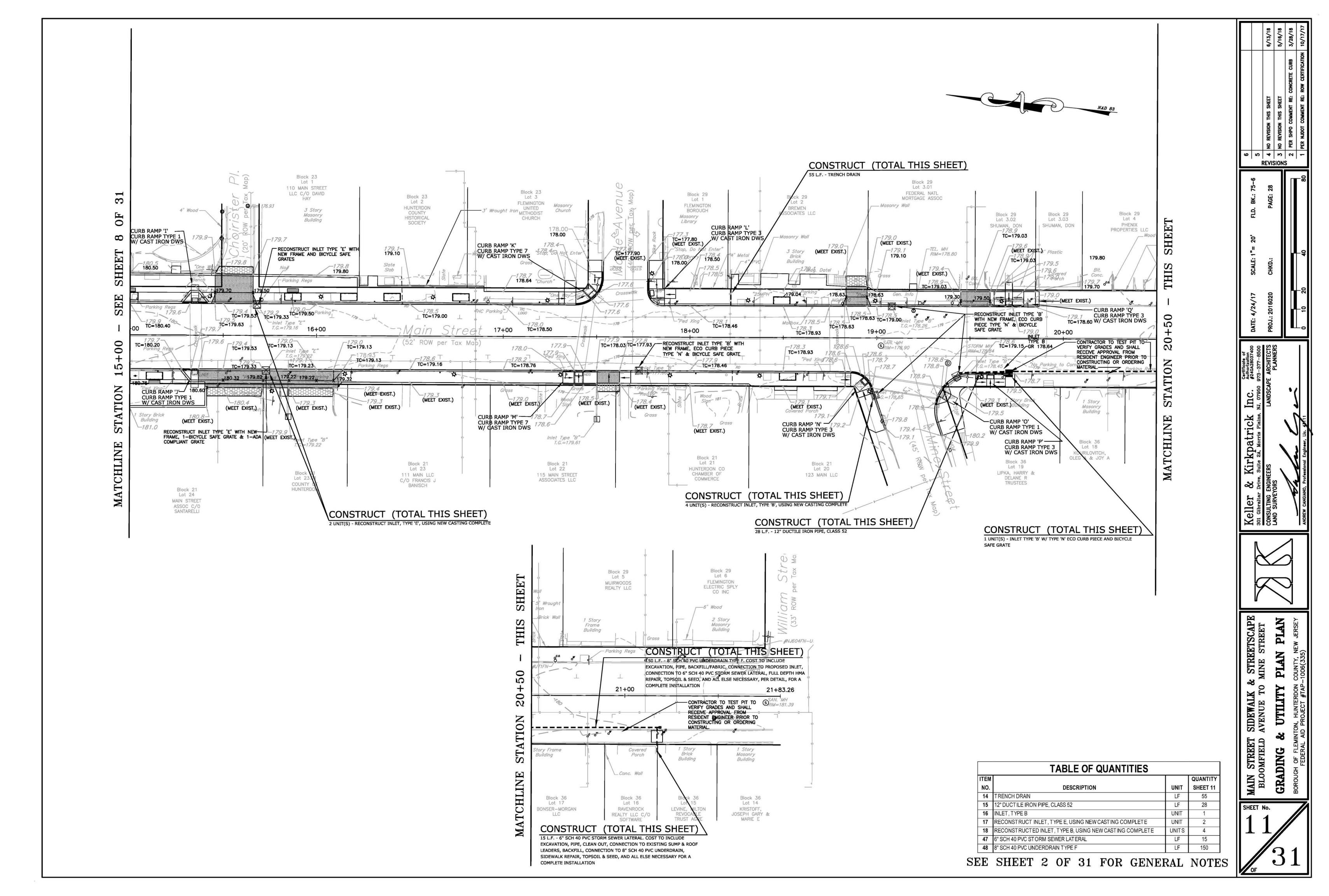


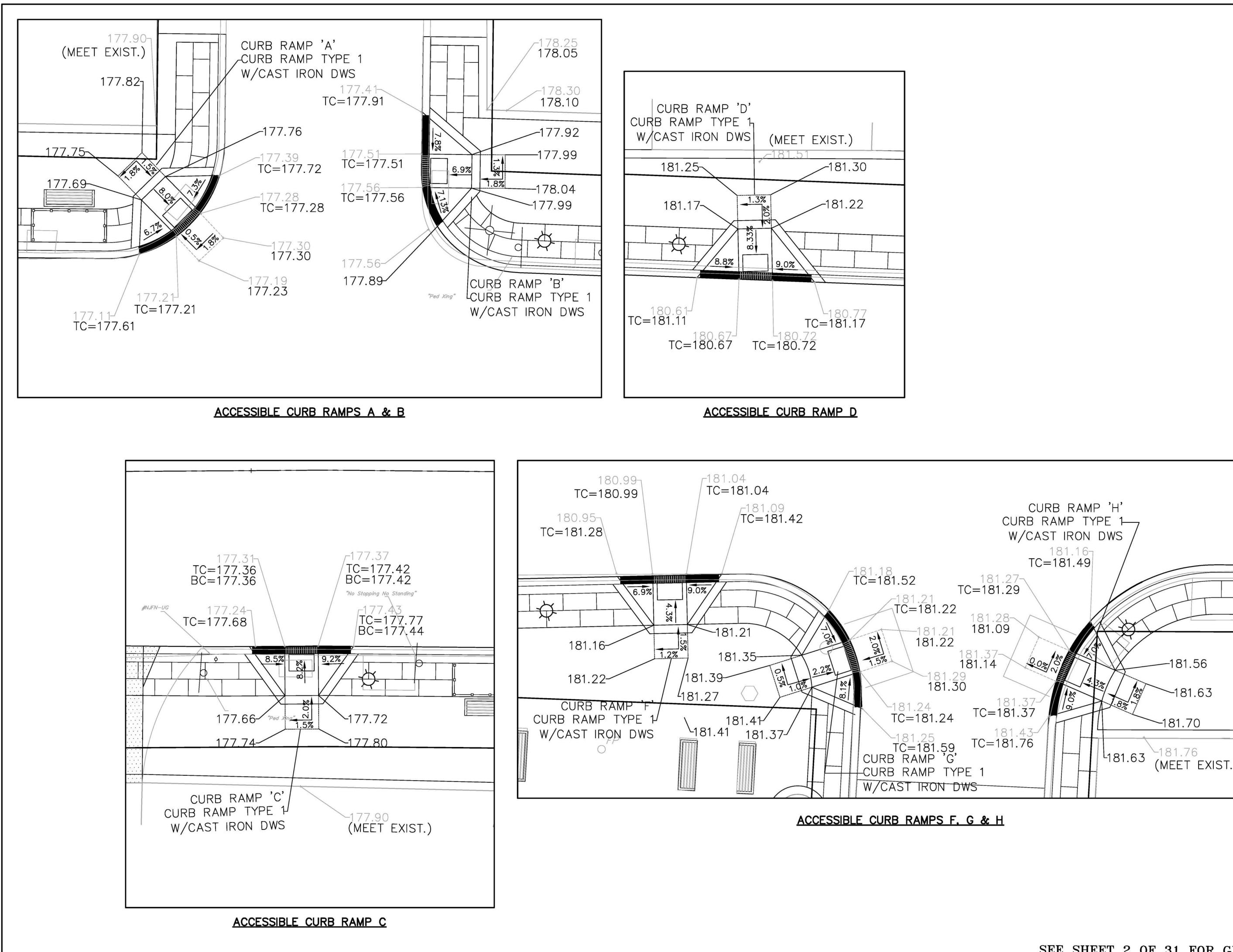
SEE SHEET 2 OF 31 FOR GENERAL NOTES

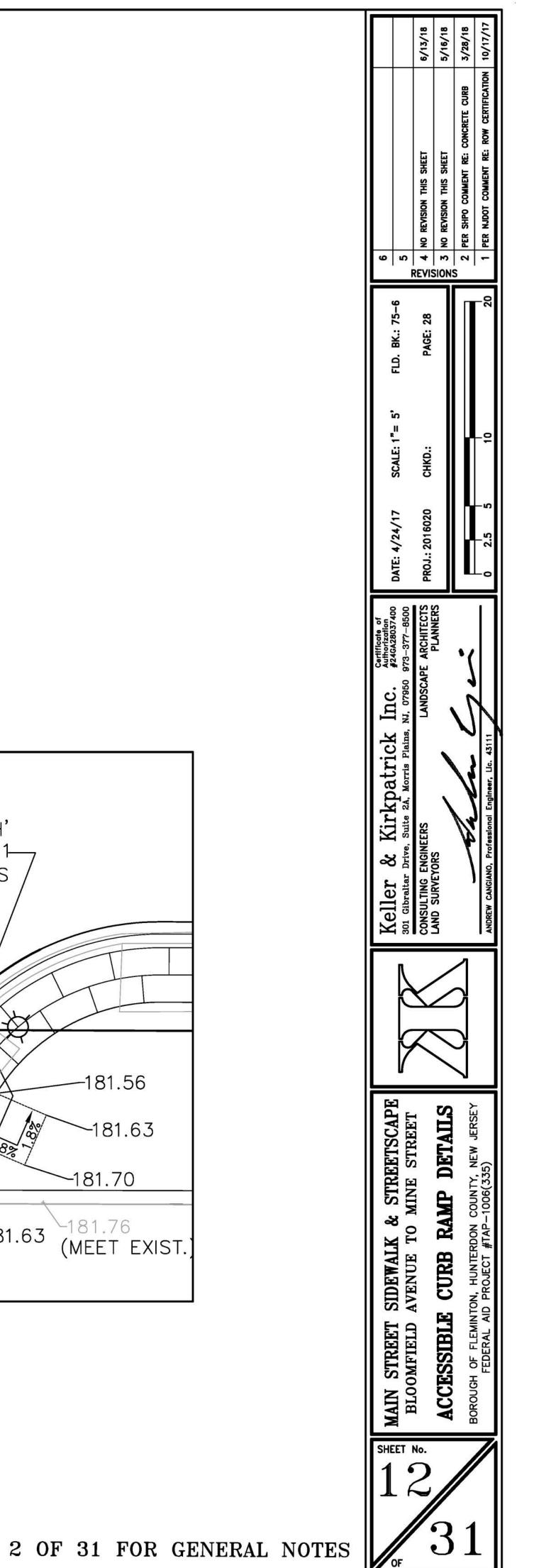
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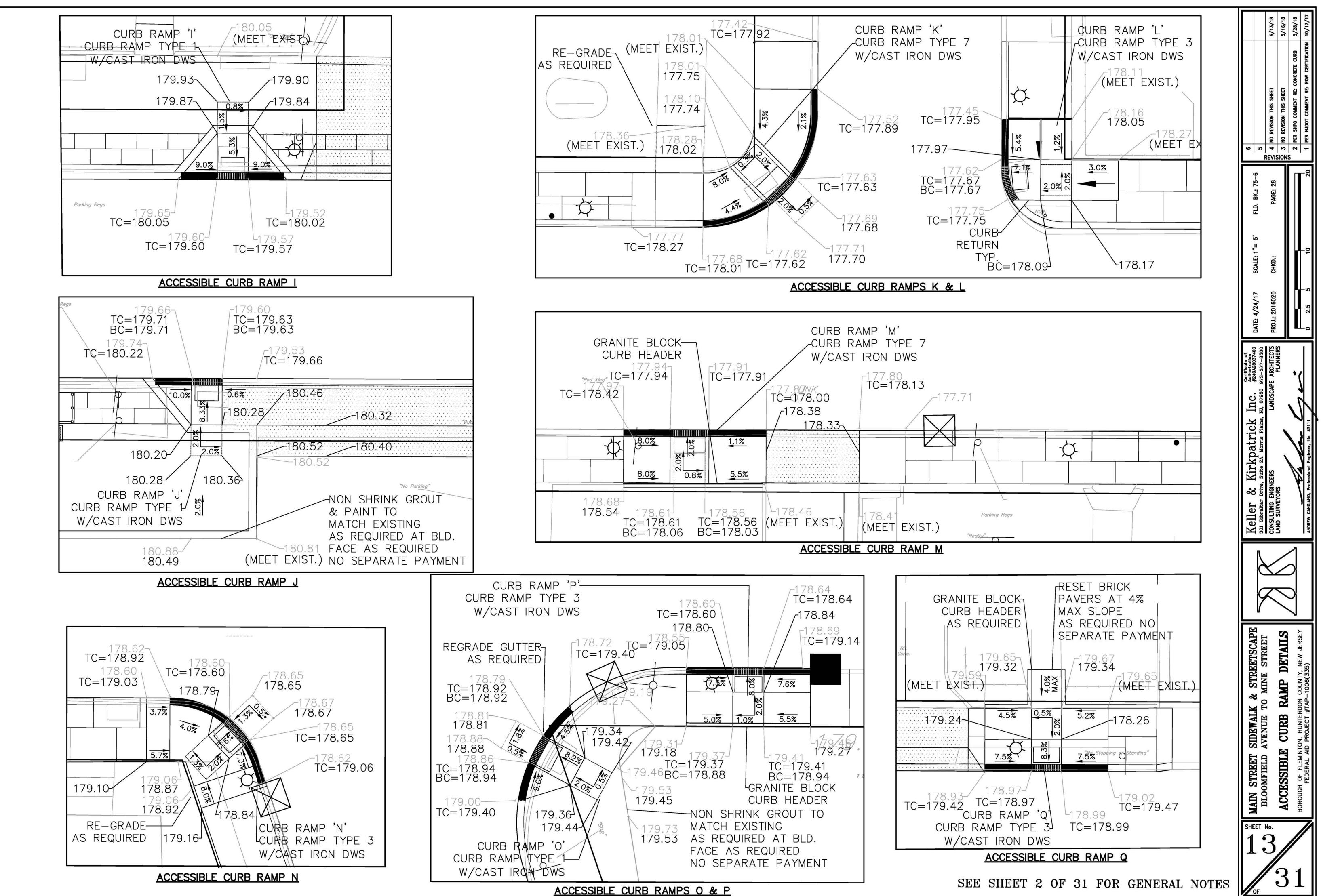












GENERAL NOTES (apply to all drawings):

- 1. VERIFY ALL ELECTRICAL REQUIREMENTS AND EXACT LOCATIONS OF EQUIPMENT WITH DRAWINGS AND SPECIFICATIONS.
- 2. SEE ALL SPECIFICATION SECTIONS FOR ADDITIONAL ELECTRICAL WORK REQUIRED, NOT COMPLETELY SHOWN ON DRAWINGS.
- 3. ALL MOUNTING HEIGHTS INDICATED ARE TO CENTERLINES OF DEVICES, EXCEPT FOR LIGHTING FIXTURES AND AS OTHERWISE NOTED OR SPECIFIED. MOUNTING HEIGHTS FOR LIGHTING FIXTURES ARE TO BOTTOM OF UNITS.
- 4. ALL 120 VOLT, 20 AMP OUTLETS LOCATED OUTDOORS SHALL HAVE GFCI PROTECTION.
- 5. PROMDE ALL GROUNDING REQUIREMENTS AS OUTLINED IN THE NATIONAL ELECTRIC CODE.
- 6. CONTRACTOR HEREBY CAUTIONED THAT ELECTRIC POWER CHARACTERISTICS (VOLTAGE, PHASE, HORSEPOWER, AMPERAGE, ETC) OF EQUIPMENT IS BASED ON AVAILABLE INFORMATION AT THE TIME OF PROJECT DESIGN. CONTRACTOR MUST VERIFY CHARACTERISTICS FOR EACH PIECE OF EQUIPMENT PRIOR TO ORDERING ELECTRICAL DEVICES. INDICATE VERIFICATION ON SUBMITTAL.
- 7. COORDINATION BETWEEN ITEMS REQUIRING ELECTRIC CONNECTIONS AND EXACT FINAL LOCATIONS MUST BE CONSIDERED PRIOR TO FINAL ROUGH IN OF DEVICES.
- 8. UPSIZE WIRE WHERE NECESSARY FOR VOLTAGE DROP PER THE LATEST EDITION OF THE NATIONAL ELECTRIC CODE.
- 9. IF NO PANEL IS DESIGNATED ON A CIRCUIT HOMERUN, OR A CIRCUIT HOMERUN IS NOT SHOWN FOR AN ITEM OF EQUIPMENT REQUIRING POWER (INCLUDING EQUIPMENT NOT SHOWN ON DRAWINGS), ELECTRICAL CONTRACTOR SHALL CIRCUIT TO THE NEAREST AVAILABLE PANEL WITH WIRE AND RACEWAY SIZED PER THE CURRENT NEC. ELECTRICAL CONTRACTOR SHALL ALSO PROMDE PROPERLY SIZED CIRCUIT BREAKER.
- 10. REFER TO SPECIFICATIONS FOR REQUIREMENTS OF COORDINATION DRAWINGS TO BE COORDINATED WITH OTHER TRADES.

		LIGHTING FI	ХТ	URE	S C	ΗΕD	ULE
TYPE	MANUFACTURER	CATALOG NUMBER		LAMPS		MOUNTING	NOTES/REMARKS
5			WATTAGE	TYPE	VOLTS	1	57
A	STERNBERG	MS805B LED MAINSTREET LUMINAIRE WITH 9400 (14') MARSHALL SERIES POLE. CAT#PT-MS805-ALED-BD/5-4ARL35T3-MDL03-HL-240- BKT-CSA-FHC-SHPA-B-8414FP5-M0DA3	66W	ЦЕД 3500К	240	POLE	 PROVIDE WITH HI-LOW CONTRO OPTION. SEE DETAIL. PROVIDE WITH INTEGRAL HOLIDA LIGHTING RECEPTACLE (GFI/WP IN-USE COVER). PROVIDE PHOTOCELL OPTION FOR (1) POLE ON EACH SIDE OF THE ROAD.

NOTES:

- 1. THE LIGHTING FIXTURES LISTED IN THIS SCHEDULE ARE THE "BASIS OF DESIGN". ANY SUBSTITUTIONS TO THOSE LISTED IN THE SCHEDULE MUST BE SUBMITTED BY THE CONTRACTOR TO THE ENGINEER, FOR REVIEW AND EITHER APPROVAL OR REJECTION,
- 2. IN ADDITION TO ALL OF THE REQUIREMENTS OF NOTE #1 ABOVE, EVERY SUBSTITUTION MUST BE PROVIDED WITH A COMPLETE TECHNICAL DESCRIPTIONS.
- 3. THE ENGINEER WILL REVIEW ONLY THOSE SUBSTITUTIONS THAT ARE SUBMITTED IN EXACT ACCORDANCE WITH NOTES #1 AND #2 ABOVE. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY AND ALL SUBSTITUTIONS.

LIGHTING FIXTURE SCHEDULE NOTES:

- 1. PROVIDE LIGHT FIXTURES AT VOLTAGE SPECIFIED IN SCHEDULE SHOWN ABOVE.
- 2. ANY LUMINAIRE OFFERED THAT IS NOT SPECIFIED IN THIS FIXTURE SCHEDULE BY MANUFACTURER AND CATALOG NUMBER SHALL BE ACCOMPANIED BY A FULL DESCRIPTION THAT INCLUDES DRAWINGS OR PHOTOGRAPHS AND INDEPENDENT TEST LAB PERFORMANCE DATA. THE PER-UNIT ADD OR DEDUCT COST SHALL ACCOMPANY THE BID.

ELECTRICAL SYMBOLS

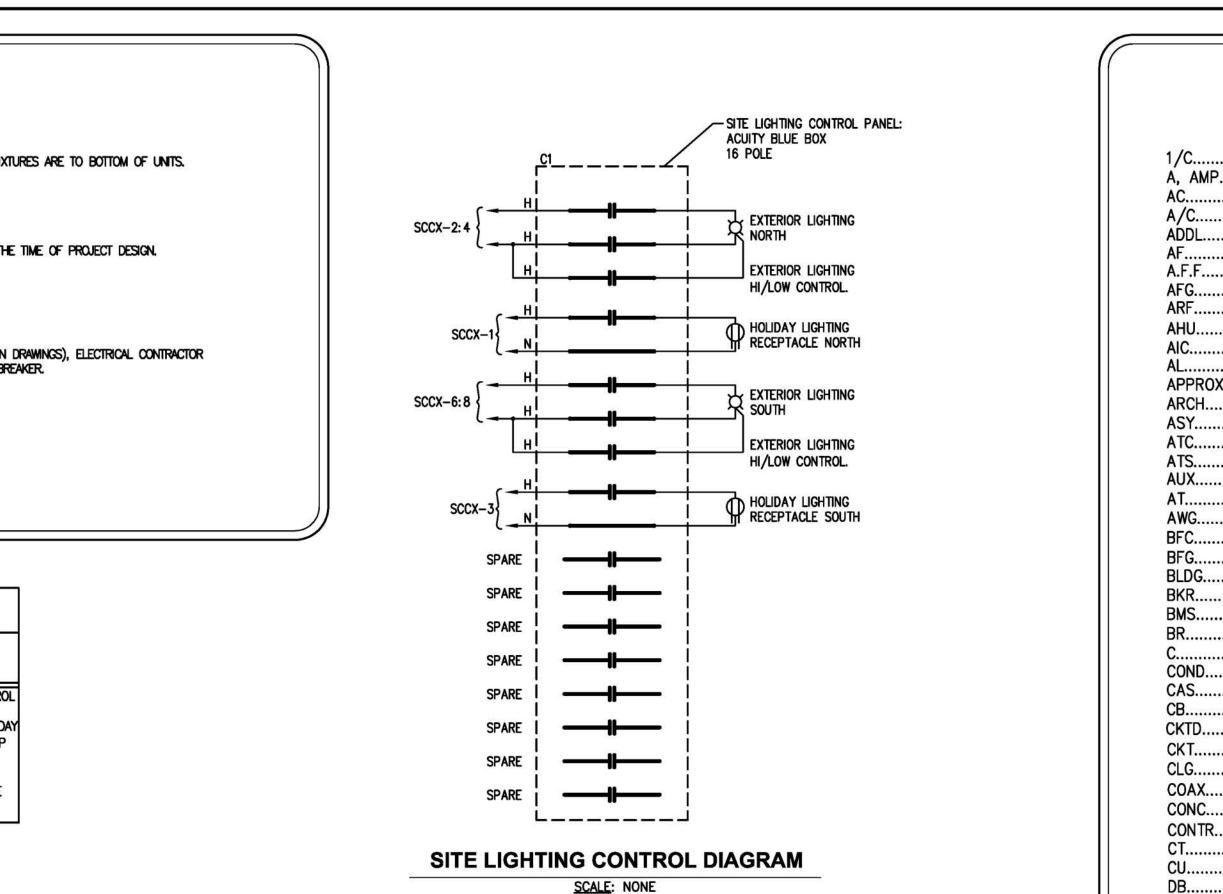
Ø	PHOTOCELL SENSOR
${}^{\rm GFI} \Phi^{\rm WP}$	TAMPER RESISTANT DUPLEX GROUND FAULT CIRCUIT INTERRUPTER 20A RECEPTACLE, WP - WEATHER PROOF DEVICE SHALL BE WEATHER PROOF "IN-USE"
J	JUNCTION BOX, WITH BLANK COVERPLATE, CEILING MOUNTED
	UNDERGROUND ELECTRICAL BRANCH CIRCUIT
¦₩ <u>~</u> LP1–9	BRANCH CIRCUIT HOMERUN WITH PANEL AND CIRCUIT NUMBER. NUMBER OF CROSSLINES INDICATE NUMBER OF BRANCH WIRES INCLUDING GROUND WIRES, EX. 2#12, 1#12 NEUTRAL, 1#12G.
\$ ^A	site lighting fixture, pole mounted. Letter indicates fixture type. See lighting fixture schedule for additional details.
ø	EXISTING OVERHEAD ELECTRIC UTILITY POLE. COORDINATE EXACT LOCATIONS IN THE FIELD.

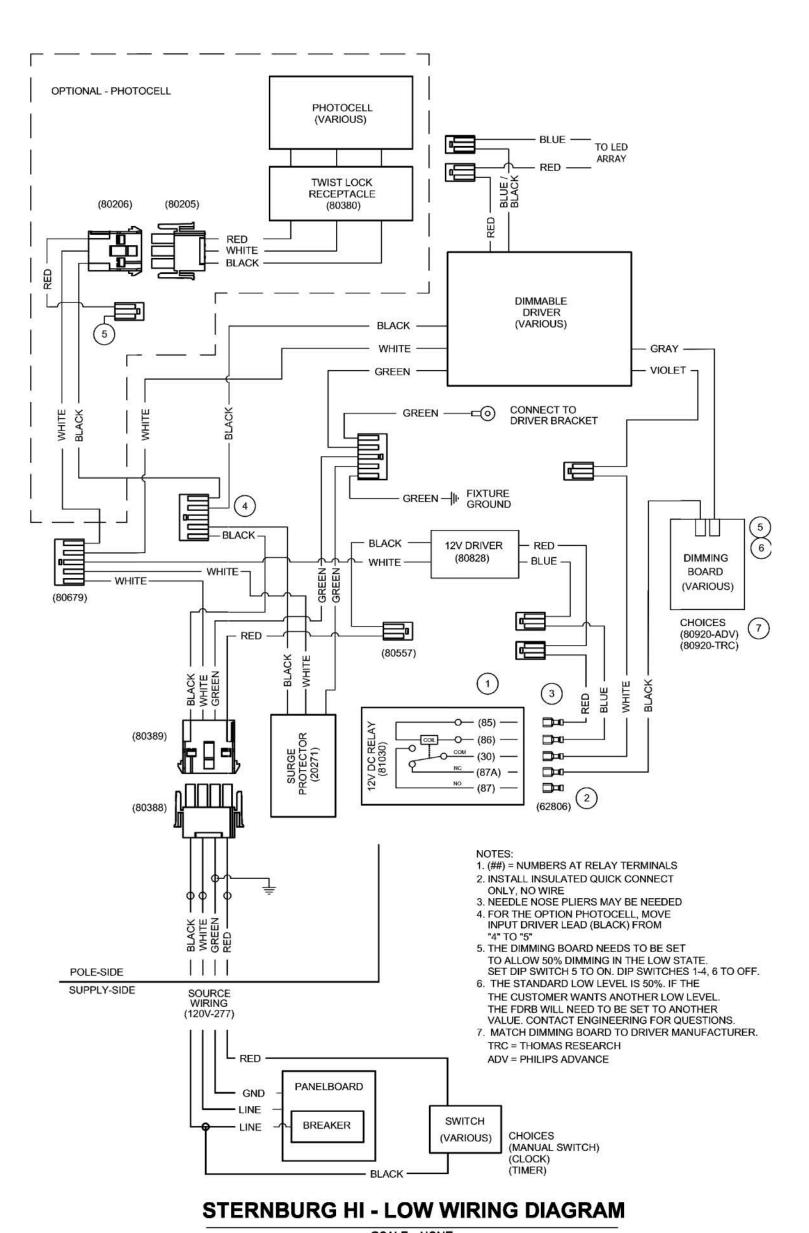
LIGHTING CONTROL DESIGN INTENT: THE CONTRACTOR SHALL FURNISH, INSTALL, AND PROGRAM THE LIGHTING CONTROL TO MEET ALL REQUIREMENTS IN THESE CONSTRUCTION DOCUMENTS AND AS STATED BELOW, UPON COMPLETION OF WORK THE CONTRACTOR SHALL DEMONSTRATE TO THE OWNER AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW, UPON SHOW DESIGN INTENT ONLY. EXACT REQUIREMENTS, DETAILS, CONTROL DIAGRAMS, MATERIALS, ETC. SHALL BE CONDINATED WITH ALL MANUFACTURERS TO PROVIDE A WORKING HOLD THE CONTROL SCIENCE ALL DESIGN FOR STATES OF AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW, UPON HOLD THE CONTROL SCIENCE ALL DESIGN FOR THE OWNER AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW. DRAWINGS HOLD THE CONTROL SCIENCE ALL DESIGN FOR THE OWNER AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW. DRAWINGS HOLD THE CONTROL SCIENCE ALL DESIGN FOR THE OWNER AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW. DRAWINGS HOLD THE CONTROL SCIENCE ALL DESIGN FOR THE OWNER AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW. DRAWINGS HOLD THE CONTROL SCIENCE ALL DESIGN FOR THE OWNER AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW. DRAWINGS HOLD THE CONTROL SCIENCE ALL DESIGN FOR THE ALL DESIGN FOR THE ALL DESIGN FOR THE ALL DRAW AND APPENDED AND APPENDED AND APPENDED FOR THE ALL DRAW AND APPENDED AND APPENDED FOR THE ALL DRAW AND APPENDED AND APPENDED AND APPENDED FOR THE ALL DRAW AND APPENDED AND APPENDED FOR THE APPENDED FOR THE APPENDED FOR THE ALL DRAW AND APPENDED AND APPENDED AND APPENDED FOR THE APPENDED FOR TH LIGHTING CONTROL SYSTEM THAT MEETS ALL REQUIREMENTS STATED BELOW. FINAL CONTROL INTENT SHALL BE SUBMITTED TO FOR FINAL REVIEW AND APPROVAL. THE OWNER RESERVES THE RIGHT TO CHANGE CONTROL INTENT TO BETTER MEET THE OWNER'S NEEDS.

- DARK 10:59PM LIGHT FIXTURES SHALL TURN ON WITH 100% LIGHT OUTPUT. POLE MOUNTED RECEPTACLES SHALL TURN ON.
- <u>11:00PM 5:00AM</u>
 UGHT FIXTURES SHALL DIM DOWN TO 50% LIGHT OUTPUT. POLE MOUNTED RECEPTACLES SHALL STAY ON.
- 5:01AM NORMAL DAYLIGHT HOURS: LIGHT FIXTURES SHALL TURN OFF.
- POLE MOUNTED RECEPTACLE SHALL TURN OFF
- LIGHT FIXTURES SHALL TURN ON WITH 100% LIGHT OUTPUT IF LIGHT LEVELS DROP (VIA PHOTOCELL).

NOTES:

- LIGHTING SCHEDULE SHALL AUTOMATICALLY ADJUST WITH TIME OF YEAR & DAYLIGHT SAVINGS TIME.
- PROMDE LOCAL OVERRIDE SWITCH IN CONTROL CABINET FOR LIGHT CIRCUITS. PROMDE LOCAL OVERRIDE SWITCH IN CONTROL CABINET FOR RECEPTACLE CIRCUITS.
- 4. PROVIDE PHOTOCELL MOUNTED ON FIRST POLE IN LIGHTING CIRCUIT FOR DAYTIME ON/OFF.







ELECTRICAL ABBREVIATIONS GROUND SENSING RELAY SINGLE CONDUCTOR GSR. 1/C..... HAND DRYER A, AMP..... AMPERE HD. ALTERNATING CURRENT HT HEIGHT HIGH INTENSITY DISCHARGE AIR CONDITIONER HP. HORSEPOWER ADDITIONAL HPS... HIGH PRESSURE SODIUM AMP FRAME HZ... HERTZ ABOVE FINISHED FLOOR INTERMEDIATE METAL CONDUIT ABOVE FINISHED GRADE IMC ABOVE RAISED FLOOR ARF..... INCH INCAND..... INCANDESCEN AIR HANDLING UNIT 8 8 8 INSULATION INTERRUPTING CAPACITY (AMPERES) INSUL.... 0 0 4 M 0 -INVERT INV..... ALUMINUM REVISIONS APPROX..... APPROXIMATELY JUNCTION BOX JB... ARCHITECTURAL THOUSAND CIRCULAR MILLS KCMIL.. KILOWATT ASYMMETRICAL KW.. AUTOMATIC TEMPERATURE CONTROL KILOWATT HOUR KWH.... AUTOMATIC TRANSFER SWITCH KVA... KILOVOLT AMPERE AUXILIARY KILOVOLT KV... AMP TRIP LONG AMERICAN WIRE GAUGE LIGHT BELOW FINISHED CEILING LTG... LIGHTING BELOW FINISHED GRADE MULTI CONDUCTOR BUILDING MOTOR CONTROL CENTER MCC BREAKER MCB. MAIN CIRCUIT BREAKER BUILDING MANAGEMENT SYSTEM MDP. MAIN DISTRIBUTION PANEL BRANCH MH MANHOLE CONDUIT MINIMUM MIN CONDUIT MLO. MAIN LUGS ONLY CARD ACCESS SYSTEM MMFO. MULTI-MODE FIBER OPTIC CIRCUIT BREAKER MTD. MOUNTED CIRCUITED NEUTRAL CIRCUIT NORMALLY CLOSED CEILING NEC. NATIONAL ELECTRIC CODE COAXIAL NATIONAL ELECTRICAL NEMA... CONCRETE à MANUFACTURERS ASSOCIATION CONTRACTOR NON-FUSED NFSD. CURRENT TRANSFORMER NOT IN CONTRACT COPPER NO. NORMALLY OPEN DIRECT BURIAL NOT TO SCALE DIRECT CURRENT F Althori #2465/ 873-3' P ARC 0.CON CENTER DISCONNECT POLE DIVISION PULLBOX DRAWING C. PHASE EACH POST INDICATING VALVE Ч, PIV. ELECTRICAL CONTRACTOR PANEL EXHAUST FAN PRIMARY PRI. ck ELEVATION PAY STATION TELEPHONE PS... irkpatri ELECTRIC POTENTIAL TRANSFORMER PT... ELEVATOR PVC... POLY VINYL CHLORIDE EMERGENCY RECEPTACLE EMERG. RCPT ELECTRICAL METALLIC TUBING **REC....** RECESSED ENCLOSURE ROOT MEAN SQUARE RMS..... ROOM EQUIP..... EQUIPMENT RM... Drive, REQ'D ... REQUIRED ELECTRIC WATER COOLER EXISTING SEC. SECONDARY er SECTION EXTERNAL / EXTERIOR SECT. SOLID NEUTRAL FLUSH SN.... Kell 301 Gibr consul FIRE ALARM ANNUNCIATOR PANEL FAAN..... SPECS SPECIFICATIONS FIRE ALARM CONTROL PANEL SUSPENDED SUSP ... FIRE ALARM SWITCH SW FAN COIL UNIT SYMMETRICAL SYM. FEEDER TELEPHONE TFI FINISHED THRU.. THROUGH FIXTURE TR.... TRIP FLOOR TVSS.. TRANSIENT VOLTAGE FULL LOAD AMPS SURGE SUPPRESSION FLEXIBLE T.S. TAMPER SWITCH FLUORESCEN' TYPICAL TYP.. FIBER OPTIC .. UNLESS OTHERWISE NOTED U.O.N. FLOW SWITCH VOLT/VOLTAGE FT..... FOOT WATT E LK & STREETSCAPE TO MINE STREET SYMBOLS, DETAILS GENERAL CONTRACTOR WITH GROUND FAULT CIRCUIT INTERRUPTER WEATHERPROOF GND, G..... GROUND XFMR..... TRANSFORMER STREET SIDEWALK MFIELD AVENUE TO ELECTRICAL SY LEGEND & DI STREET OMFIELD GREENMAN - PEDERSEN, INC. MAIN BLO 50 glenmaura national blvd, suite 102 scranton pa, 18505 tel 570.342.3700 [] fax 570.342.3080

DC..

DISC.

DIV...

DWG.

EA...

E.C.

ELEC...

ELEV

EMT.....

ENCL.

EWC. EXIST.

EXT..

FACP..

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FCU.

FDR.

FIN...

FIXT.

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GFCI.

FLEX....

FLUOR.

EF.

GPI Certificate of Authorization: 24GA27959500

SHEET No.



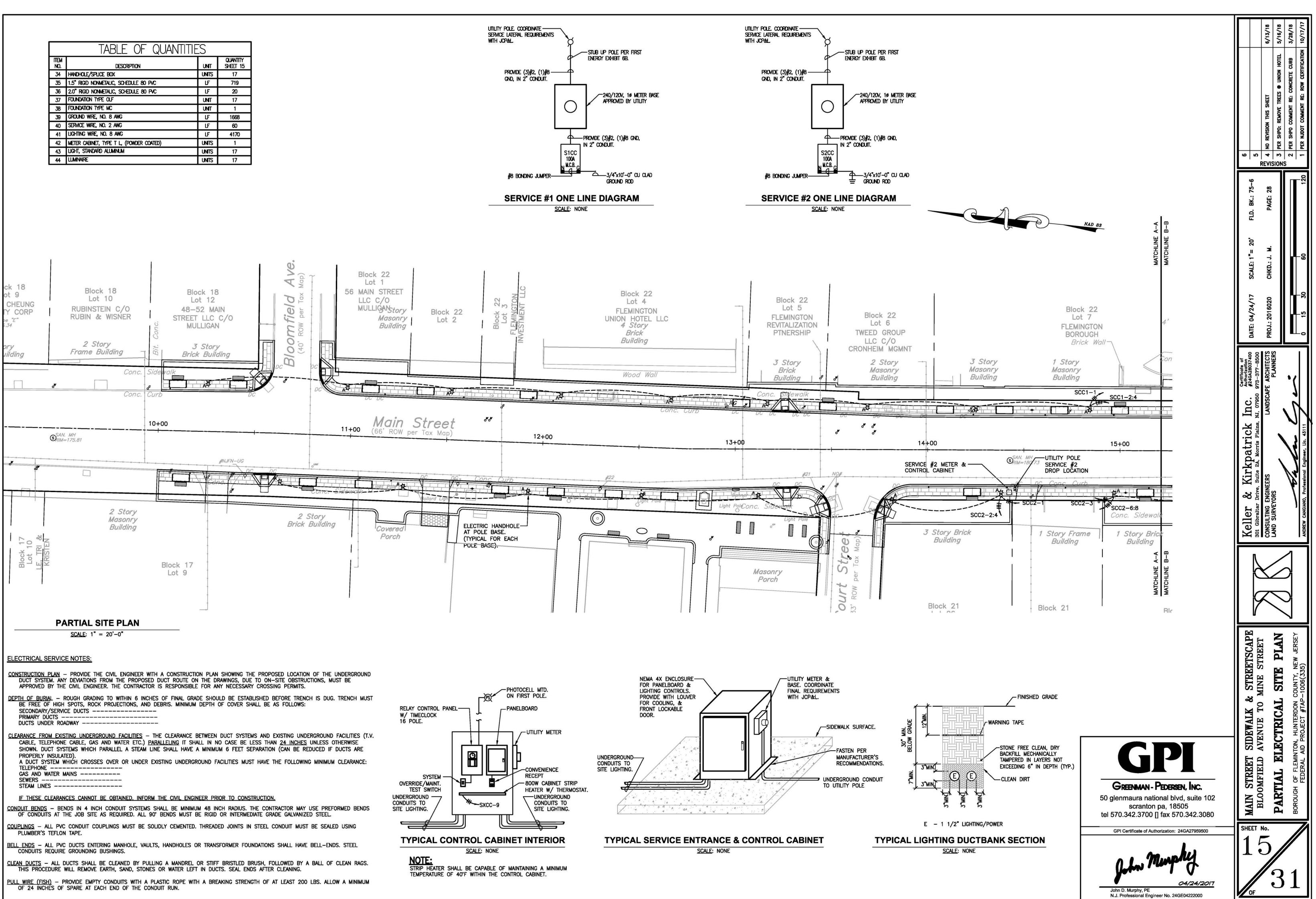
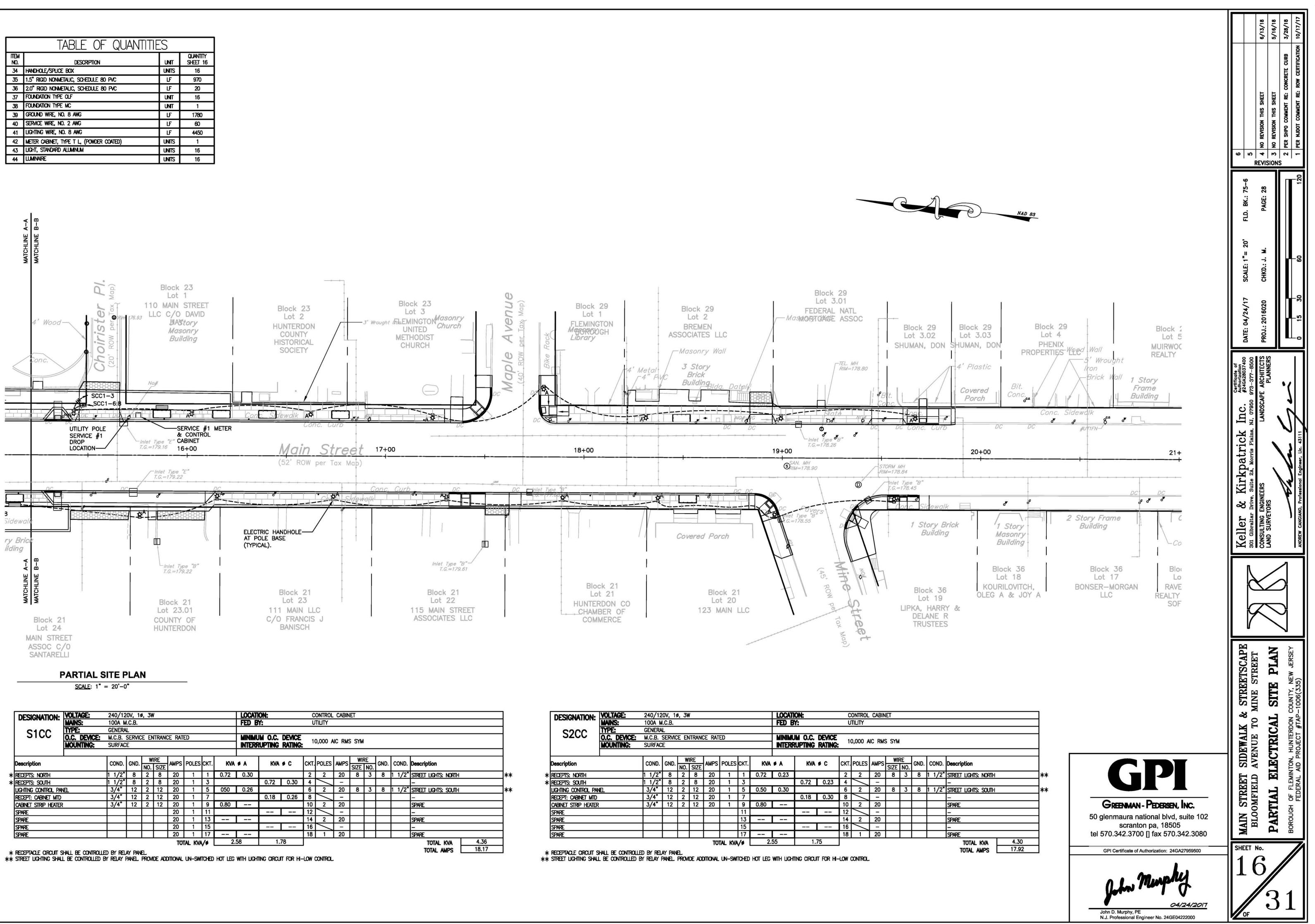
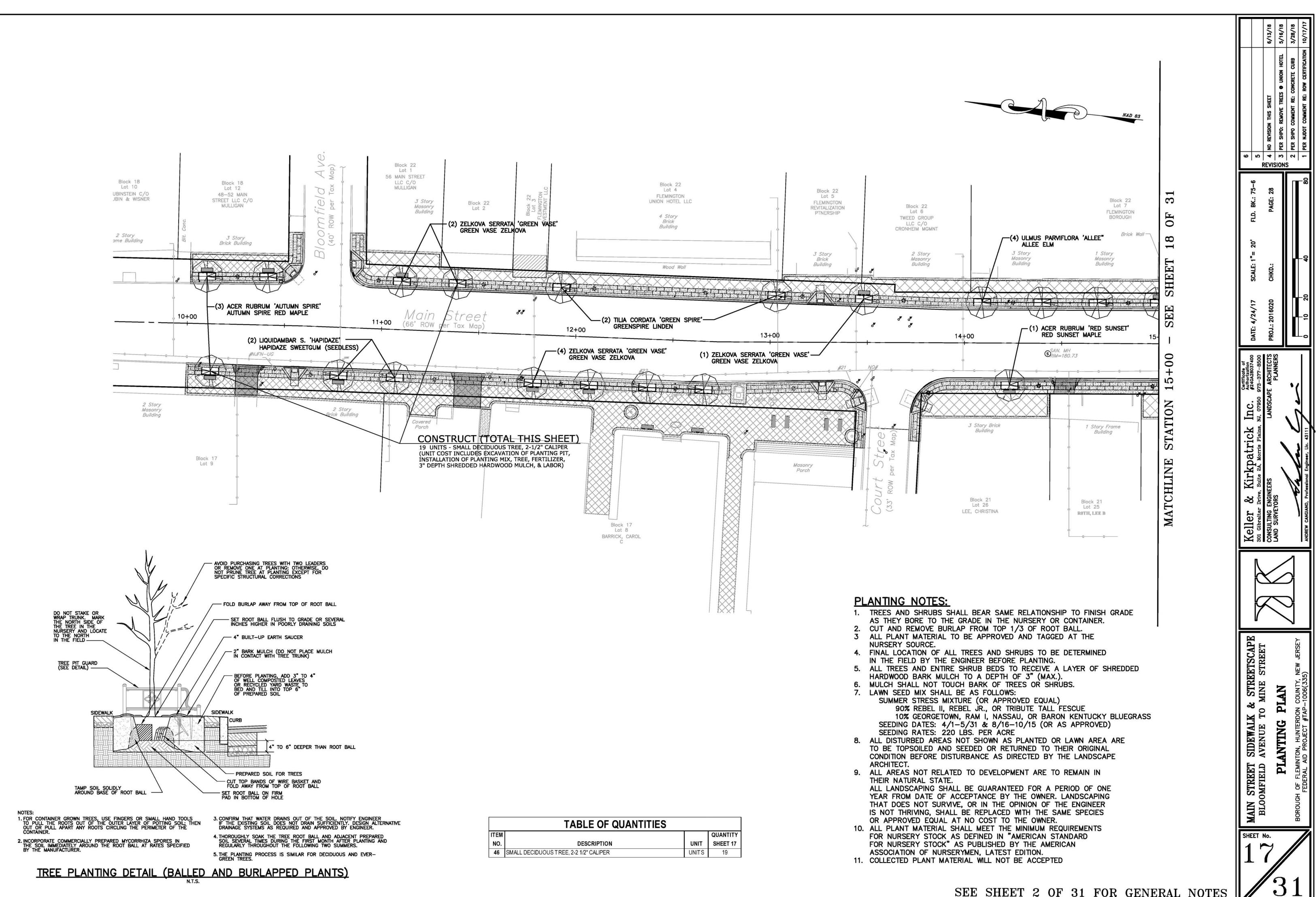


	TABLE OF QUANTI	TIES	
item No.	DESCRIPTION	UNIT	QUANTITY SHEET 16
34	HANDHOLE/SPLICE BOX	UNITS	16
35	1.5" RIGID NONMETALIC, SCHEDULE 80 PVC	Ŀ	970
36	2.0" RIGID NONMETALIC, SCHEDULE 80 PVC	Ŀ	20
37	Foundation type olf	UNIT	16
38	FOUNDATION TYPE MC	UNIT	1
39	GROUND WIRE, NO. 8 AWG	Ŀ	1780
40	SERVICE WIRE, NO. 2 AWG	Ŀ	60
41	LIGHTING WIRE, NO. 8 AWG	Ŀ	4450
42	METER CABINET, TYPE T L, (POWDER COATED)	UNITS	1
43	LIGHT, STANDARD ALUMINUM	UNITS	16
44	LUMNARE	UNITS	16

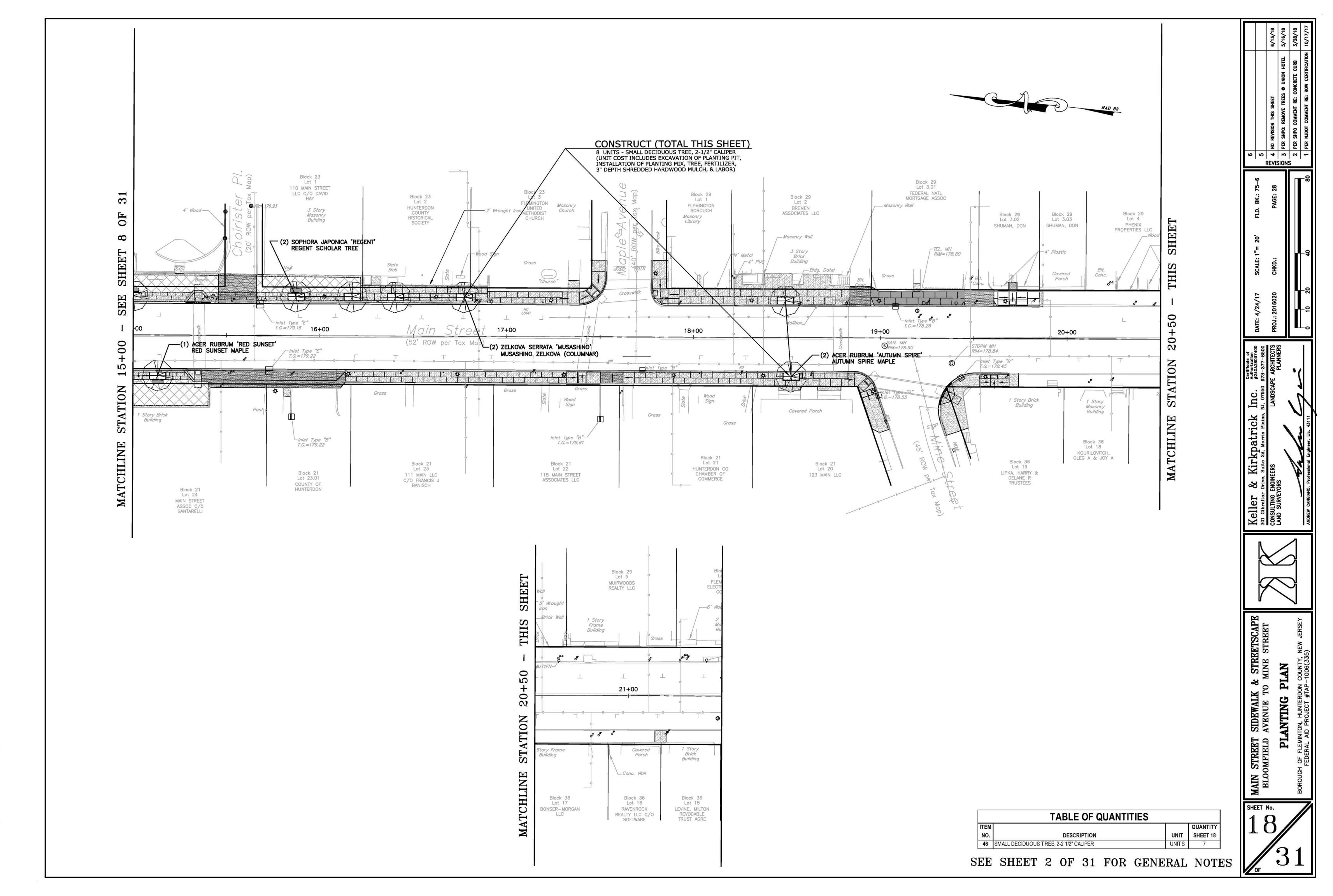


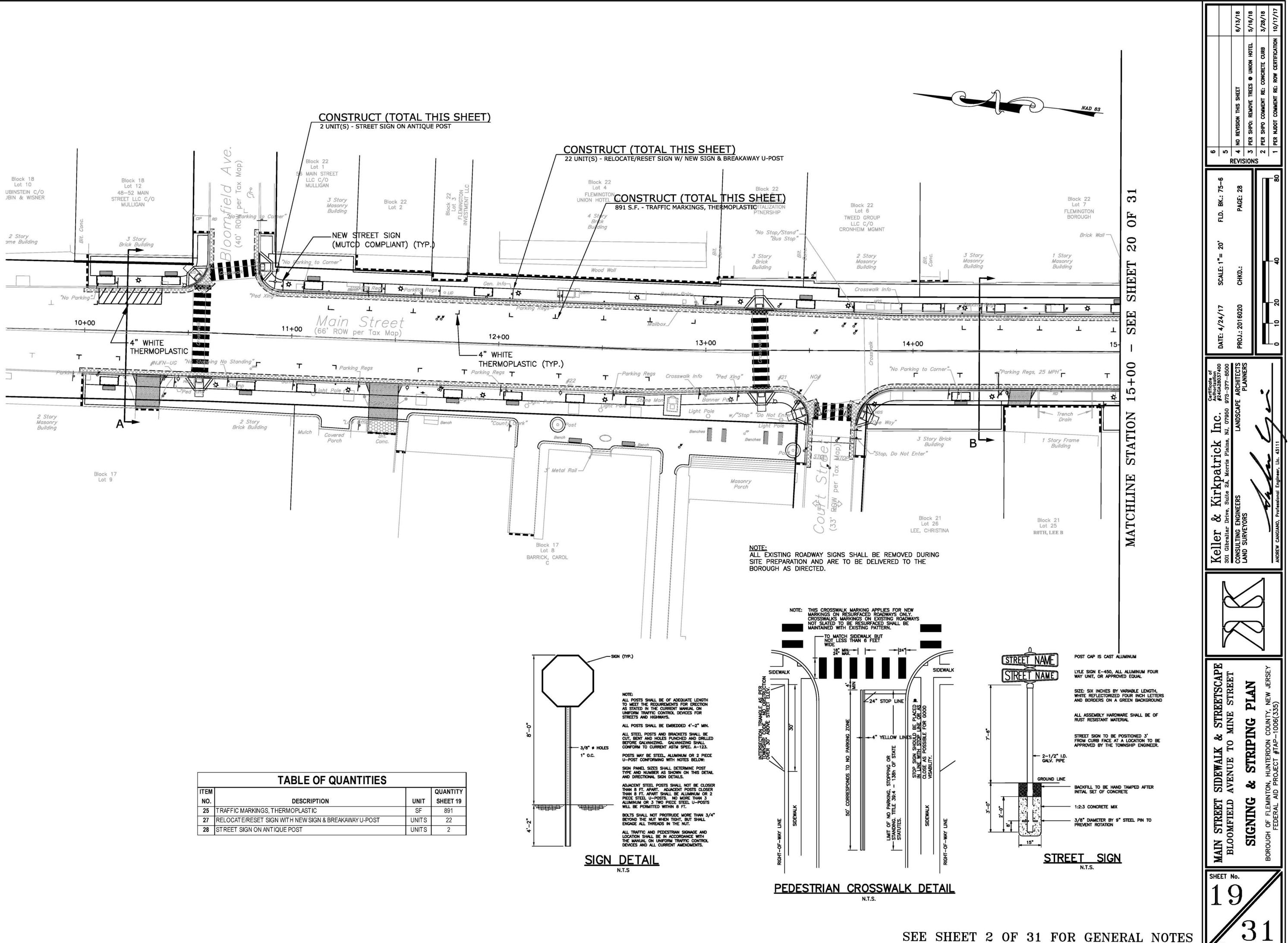
DESIGNATION:	VOLTAGE:	240/12	OV, 1	ø, 3W	1					LOCAT	ON:		C	ONTROL	. CABIN	NET						
	MAINS:	100A M	I.C.B.							FED B	Y:		ι	TILITY								
C100	TYPE:	GENERA	L																			
S1CC	O.C. DEVICE:	M.C.B.	SERVIC	je en	TRAN	CE RAT	ED			MINIMU	JM O.C.	DEVICE	4	0,000 A								
	MOUNTING:	SURFAC	E							INTER	RUPTING	RATING	: '	0,000 7		3 31						
10.000	6 T		1200120	w	IRE				04120		0000	10 1020		1999 - 1995 1995 - 1995		w	RE			La 19792		
escription		COND.	GND.		SIZE	AMPS	POLES	скт.	KVA	ø A	KVA	¢C	СКТ.	POLES	AMPS	SIZE		GND.	COND.	Description		
CEPTS: NORTH		1 1/2"	8	2	8	20	1	1	0.72	0.30			2	2	20	8	3	8	1 1/2"	STREET LIGHTS: NORTH		
ECEPTS: SOUTH		1 1/2"	8	2	8	20	1	3			0.72	0.30	4	/	-					-		
GHTING CONTROL PAN	NEL.	3/4"	12	2	12	20	1	5	050	0.26			6	2	20	8	3	8	1 1/2"	STREET LIGHTS: SOUTH		
CEPT: CABINET MID		3/4"	12	2	12	20	1	7			0.18	0.26	8	/	2-2-3					-		
BINET STRIP HEATER		3/4"	12	2	12	20	1	9	0.80	200			10	2	20					SPARE		
ARE						20	1	11					12	/	-					=		
ARE						20	1	13					14	2	20					SPARE		
ARE			5			20	1	15					16	/	-							
PARE						20	1	17					18	1	20					SPARE		
						TO	TAL KV	Vø	2	.58	1.3	78								TOTAL KVA	4.36	
ECEPTACLE CIRCUIT S				-									-							TOTAL AMPS	18.17	

DESIGNATION:	VOLTAGE:	240/12	20V, 1	ø, 3V	٧					LOCAT	ON:		0	CONTROL	. CABIN	IET
	MAINS:	100A N	I.C.B.							FED B	Y:		Ľ	JTILITY		
S2CC	TYPE:	GENER	۲.													
5200	O.C. DEVICE:	M.C.B.	SERVIC	e en	TRAN	CE RAT	ED			MINIM	JM O.C.	DEVICE	4	0,000 A		c c'
	MOUNTING:	SURFAC	Ж							INTERF	RUPTING	RATING	: '	0,000 /		33
Description		COND.	GND.	_	IRE	AMPS	POLES	CKT	KV/A	ø A	KVA	ø C	CKT	POLES		
Description				NO.	SIZE	AMES	FULLS	UNI.	NA	7 7		v U	UNI	FULLS	AMP 3	SIZ
RECEPTS: NORTH		1 1/2"		2	8	20	1	1	0.72	0.23			2	2	20	8
RECEPTS: SOUTH		1 1/2"	8	2	8	20	1	3			0.72	0.23	4	\geq	-	
LIGHTING CONTROL PA	NEL	3/4"	12	2	12	20	1	5	0.50	0.30			6	2	20	8
RECEPT: CABINET MTD		3/4"	12	2	12	20	1	7			0.18	0.30	8	/	-	
CABINET STRIP HEATER	2	3/4"	12	2	12	20	1	9	0.80				10	2	20	
SPARE								11					12	/	-	
SPARE								13					14	2	20	
SPARE							·	15					16	\backslash	-	
SPARE					[17					18	1	20	
90						TO		14	2	55	1	75				



SEE SHEET 2 OF 31 FOR GENERAL NOTES

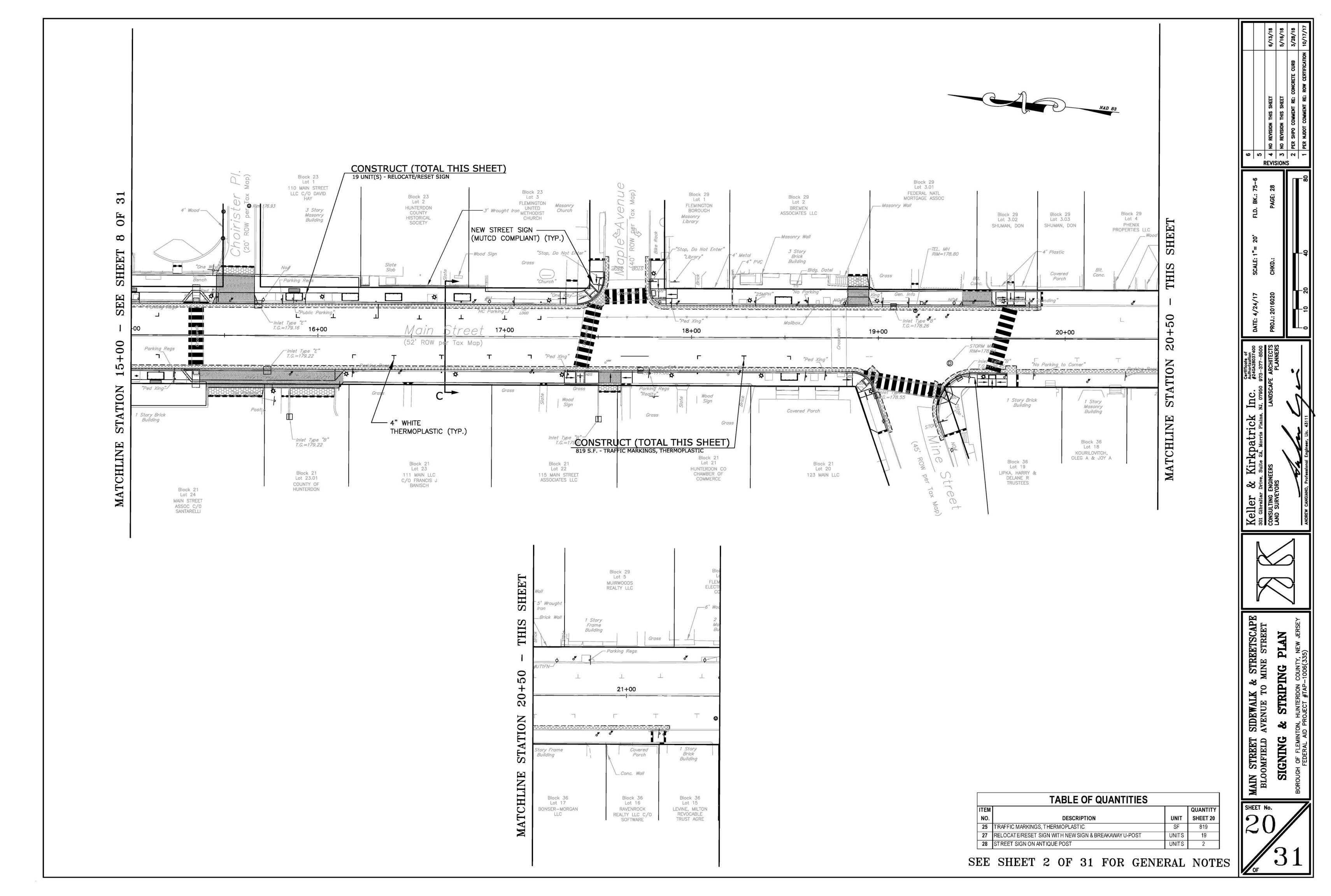


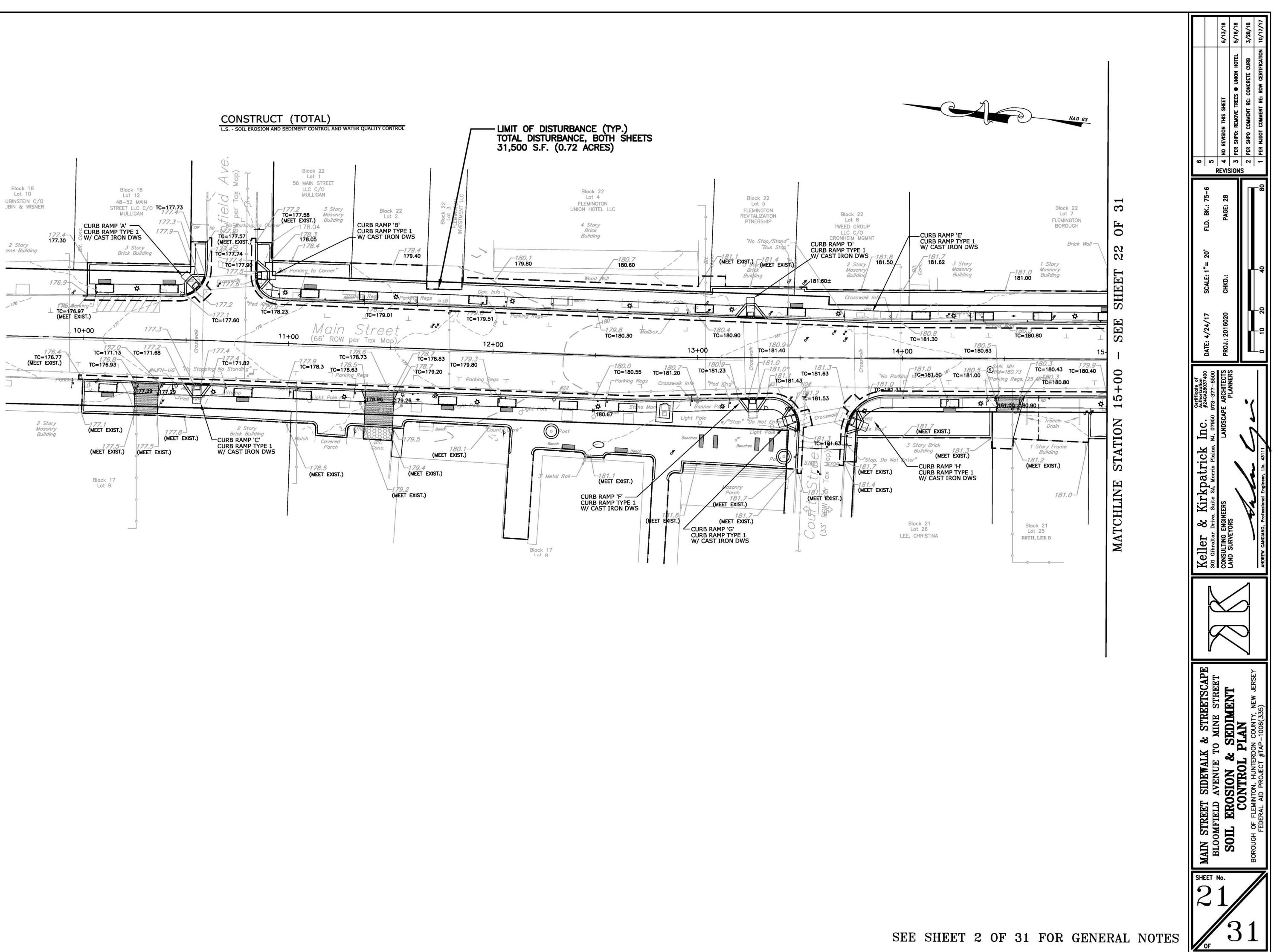


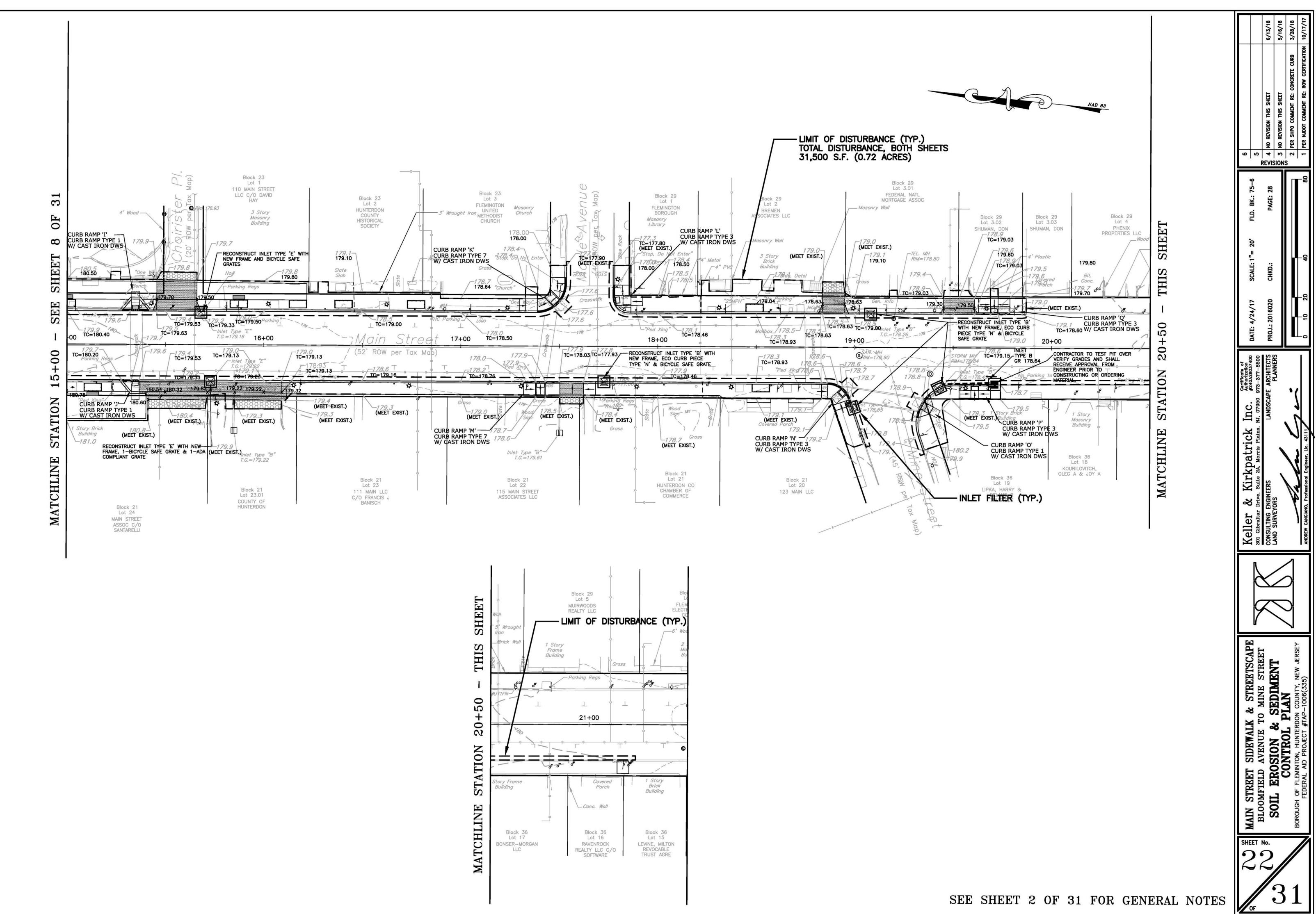
EM	
ю.	DESCRIPTION
25	TRAFFIC MARKINGS, THERMOPLASTIC
27	RELOCATE/RESET SIGN WITH NEW SIGN & BR
28	STREET SIGN ON ANTIQUE POST

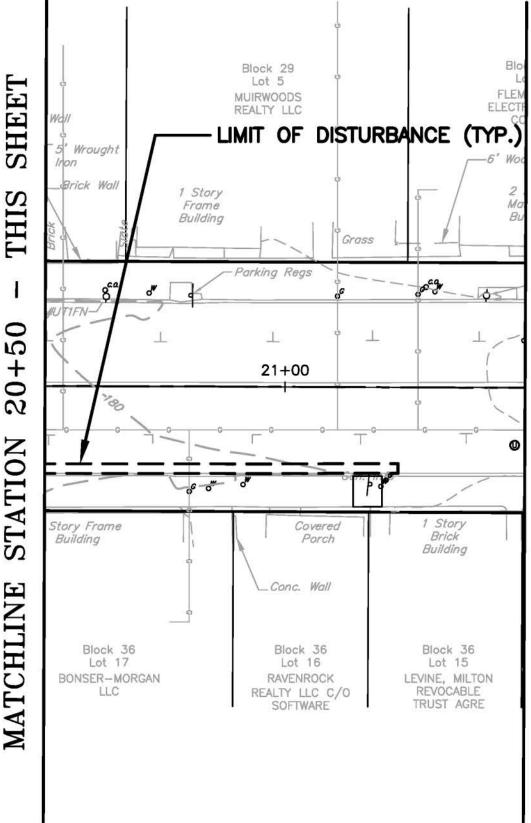
SEE SHEET 2 OF 31 FOR GENERAL NOTES

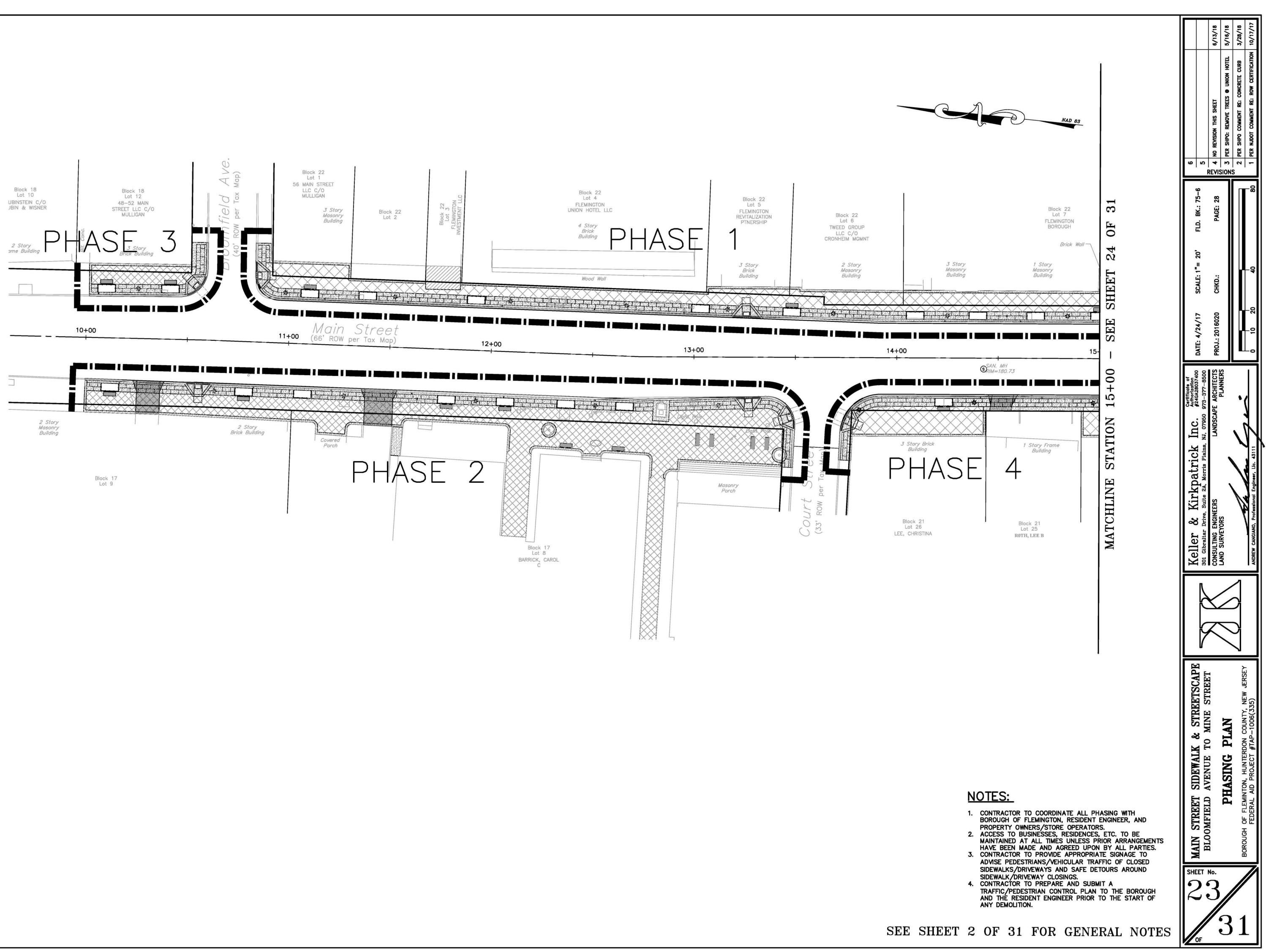
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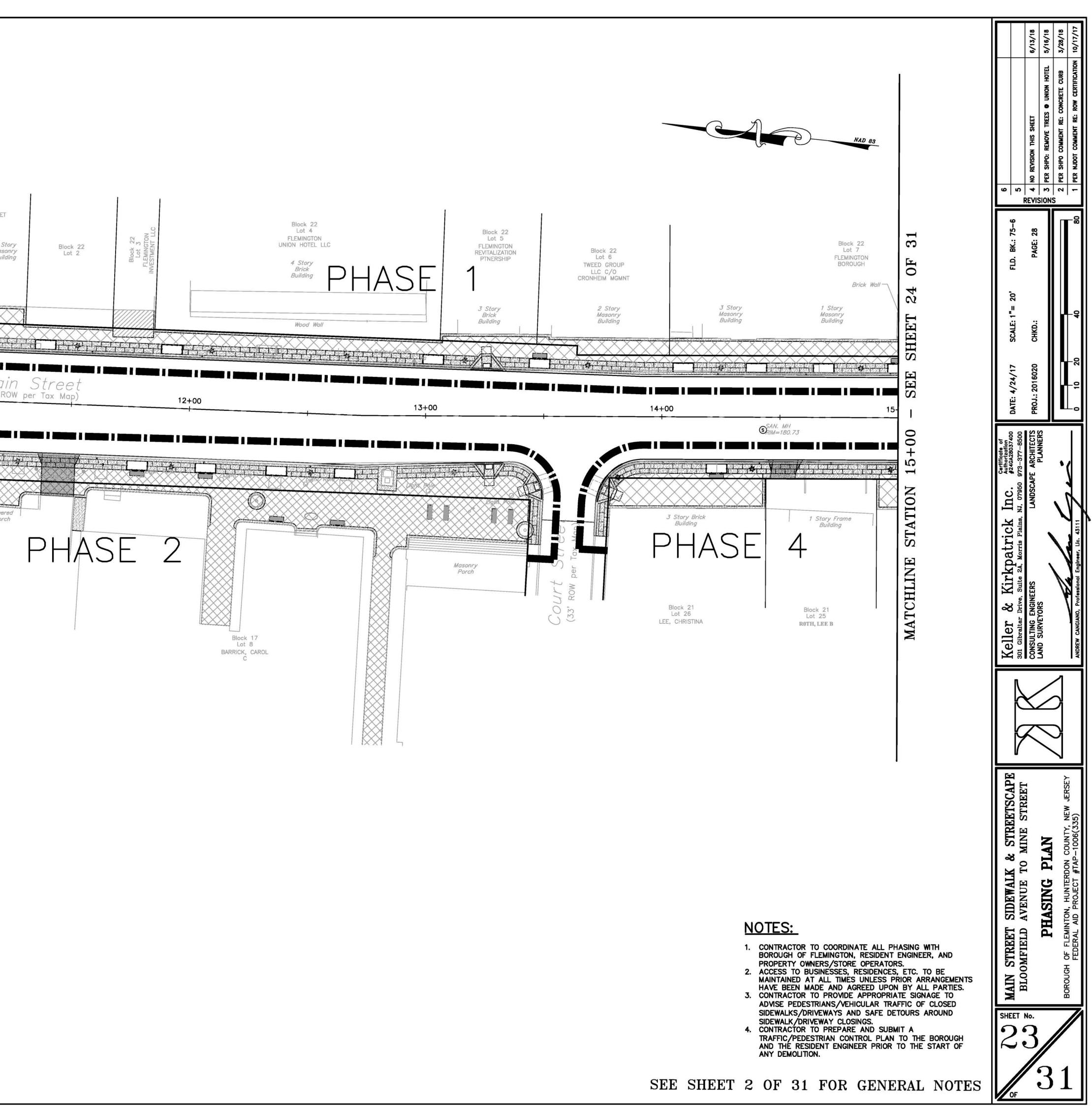


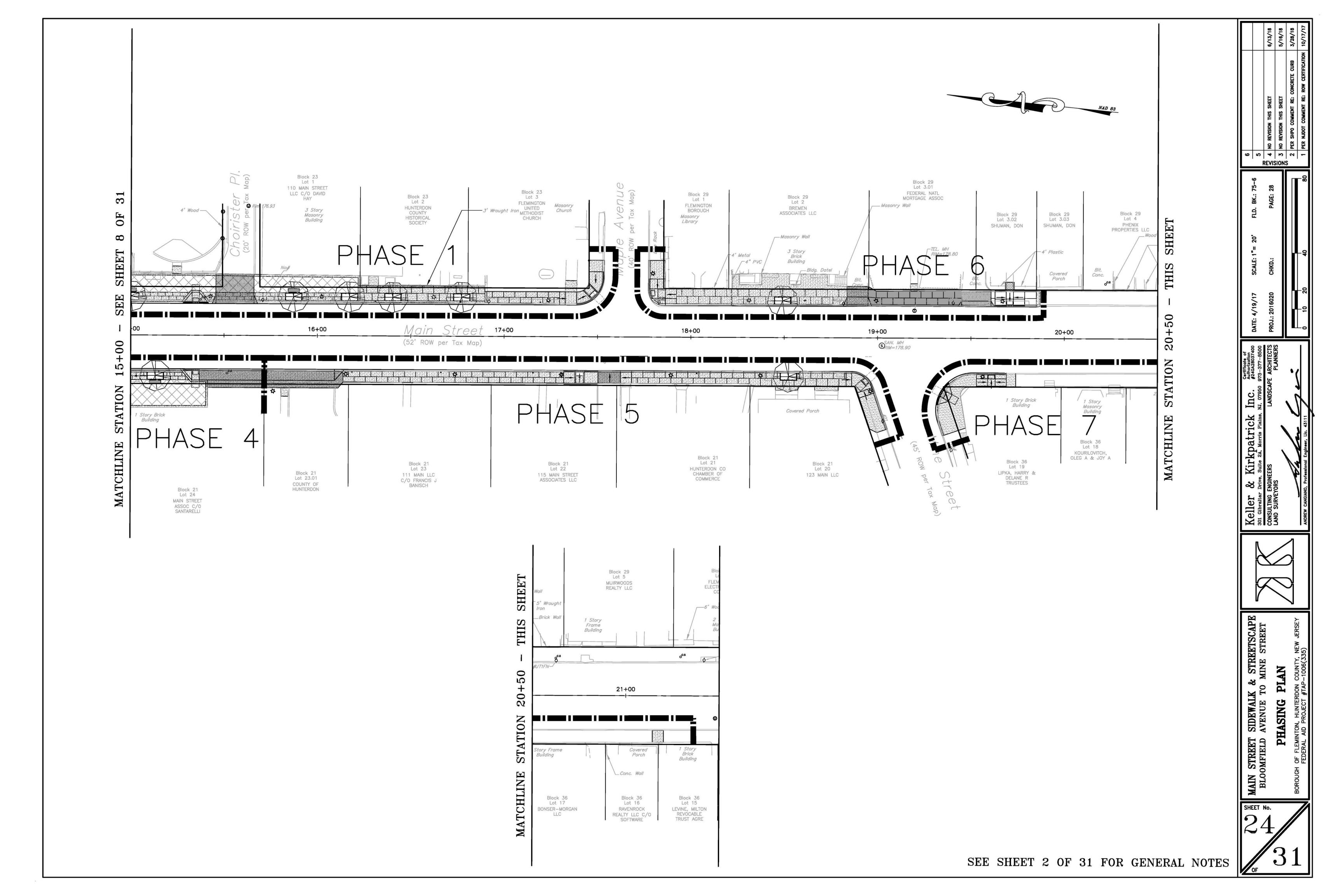


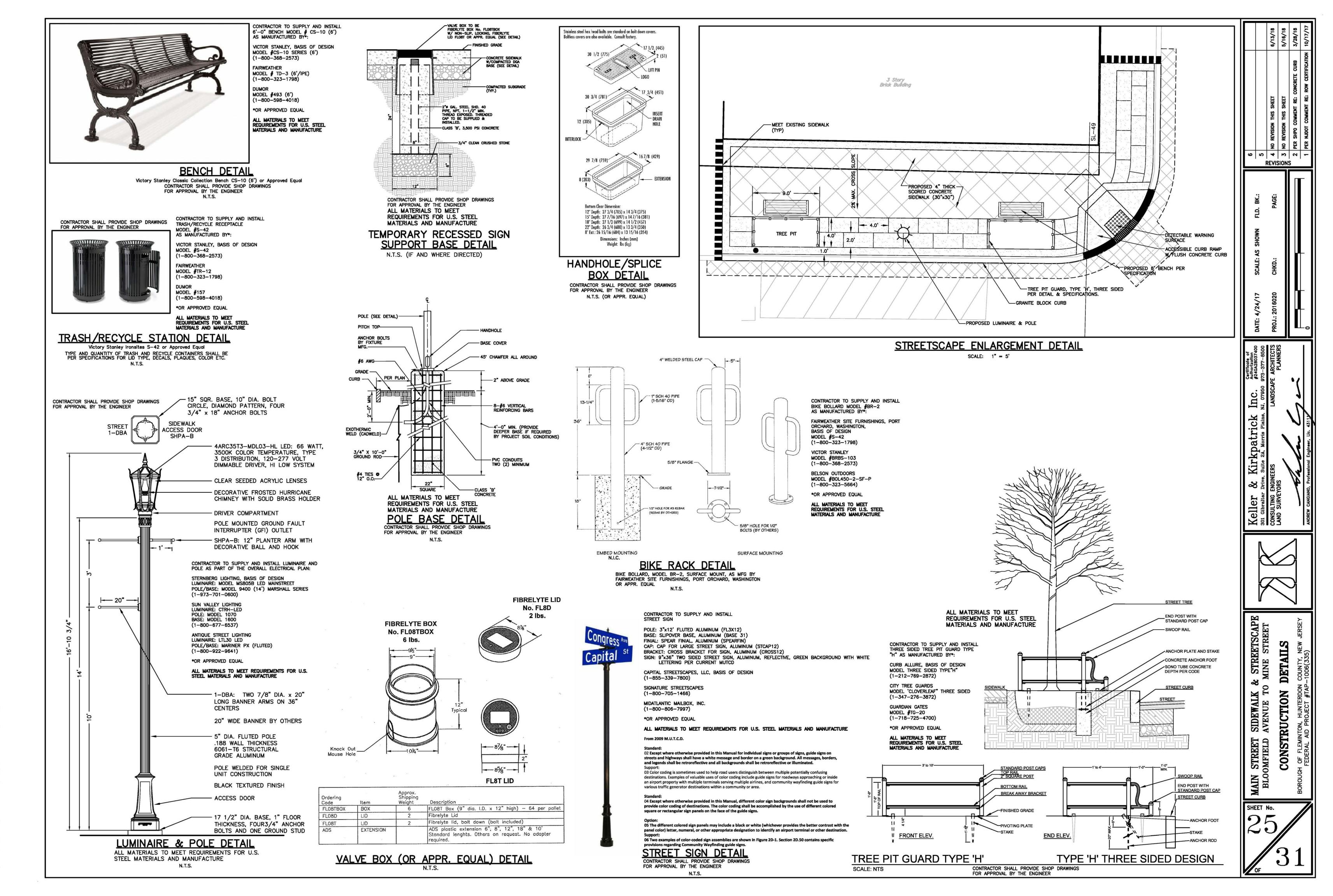


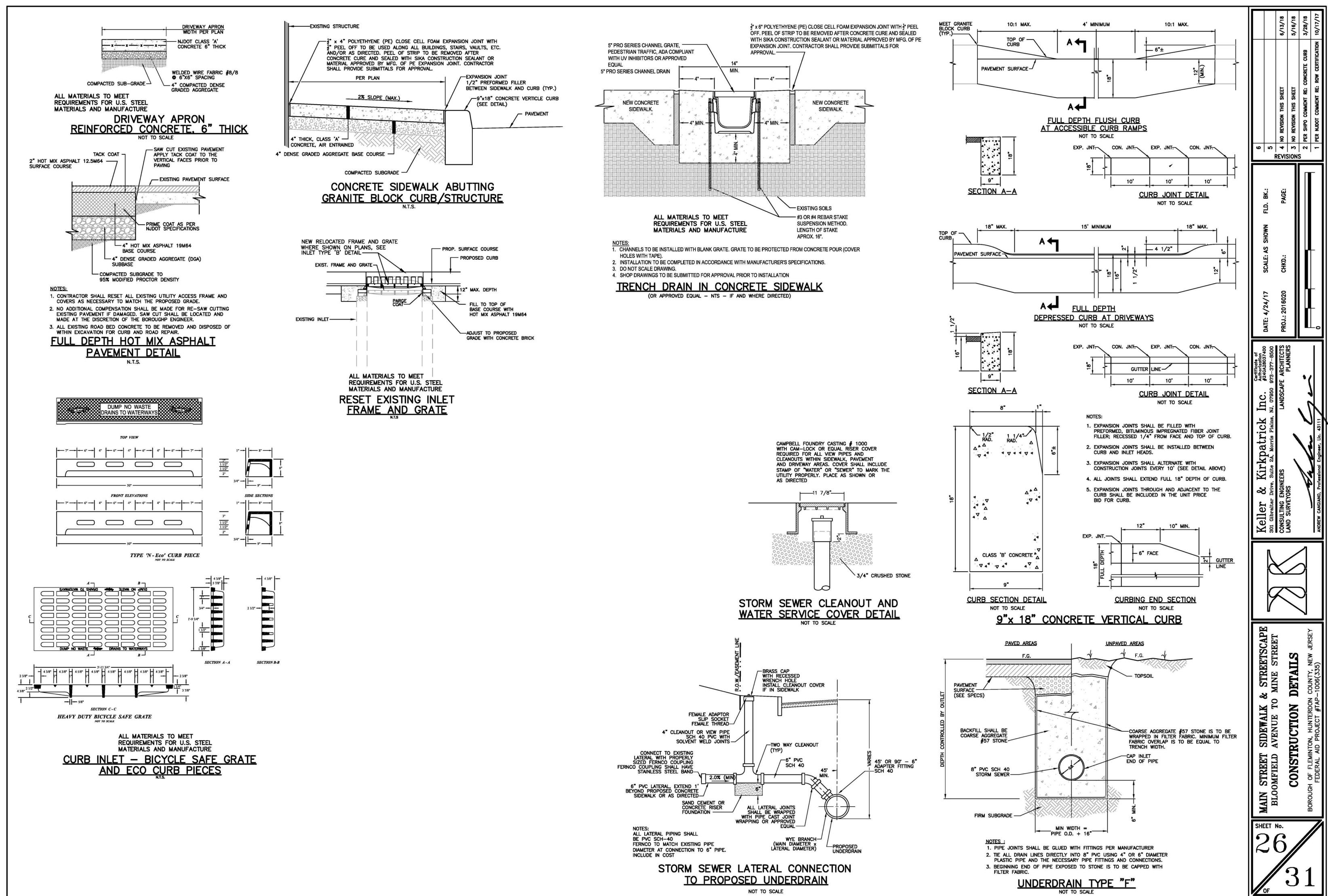


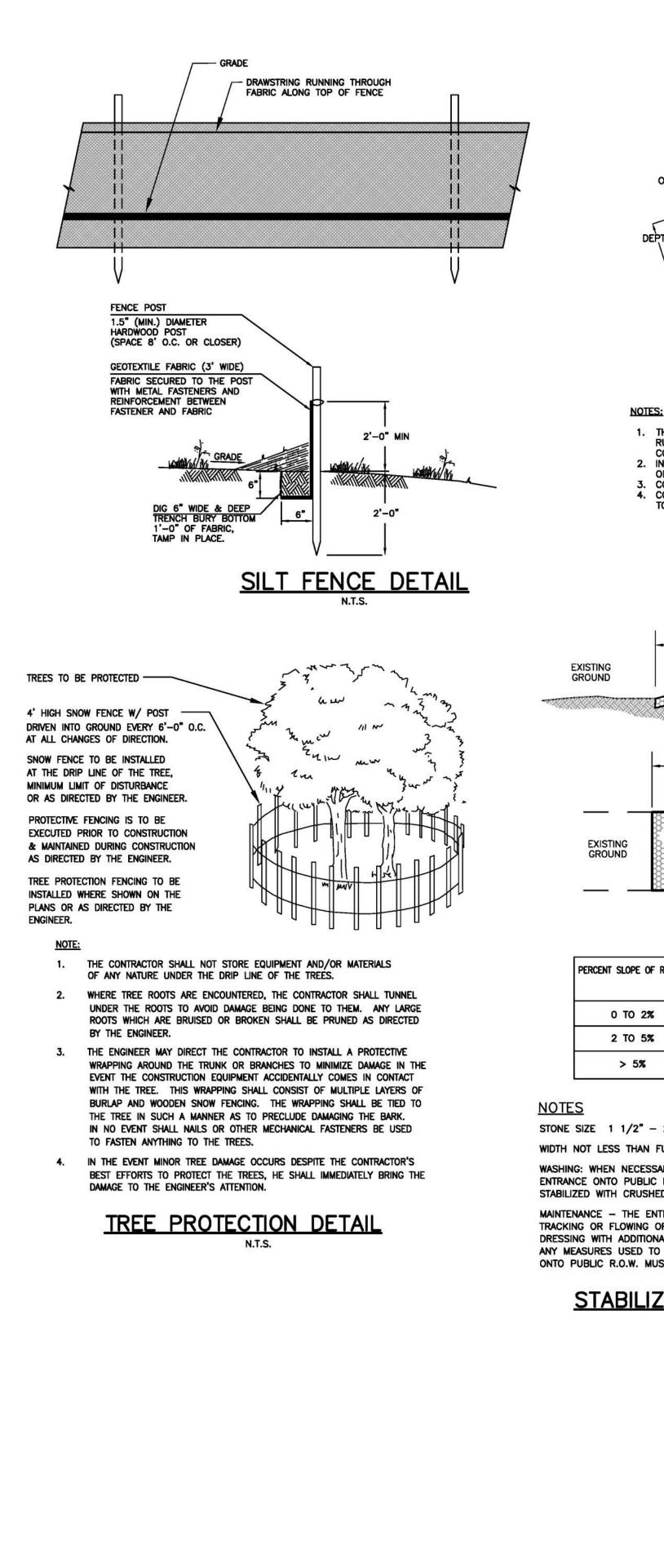


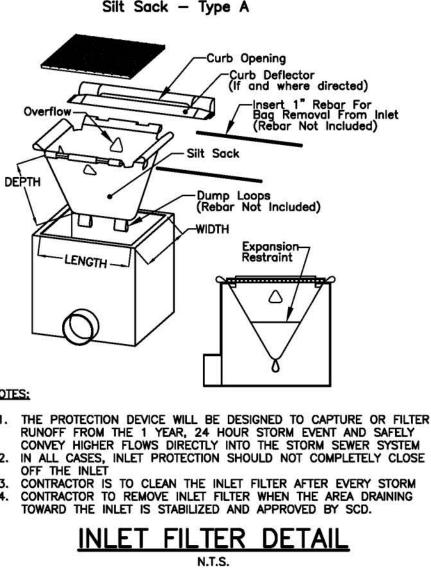


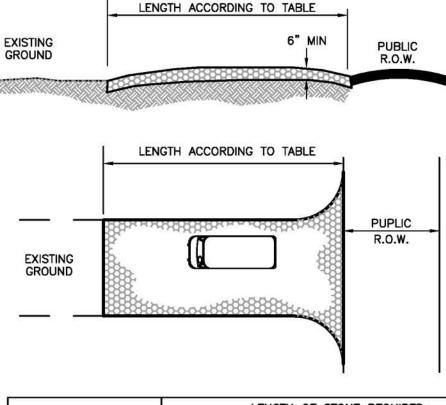












PERCENT SLOPE OF ROADWAY	LENGTH OF STONE REQUIRED						
	COURSE GRAINED SOILS	FINE GRAINED SOILS					
0 TO 2%	50 FT.	100 FT.					
2 TO 5%	100 FT.	200 FT.					
> 5%	ENTIRE SURFACE STABILIZED	WITH FABC BASE COURSE					

STONE SIZE 1 1/2" - 2 1/2" CRUSHED STONE WIDTH NOT LESS THAN FULL WIDTH AT POINTS OF EGRESS AND INGRESS.

WASHING: WHEN NECESSARY, WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC R.O.W.. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA

STABILIZED WITH CRUSHED STONE WHICH DRAINS INTO AN APPROVED SEDIMENT BASIN. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC R.O.W.. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRAPPED ONTO PUBLIC R.O.W. MUST BE REMOVED IMMEDIATELY.

STABILIZED CONSTRUCTION ENTRANCE

N.T.S - IF AND WHERE DIRECTED

HUNTERDON COUNTY SOIL CONSERVATION DISTRICT SOIL EROSION & SEDIMENT CONTROL NOTES

INLETS.

- 1. THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT REQUIRES AN ADVANCED 48-HOUR WRITTEN NOTIFICATION PRIOR TO THE START OF ANY LAND DISTURBANCE. A FAILURE OF THIS NOTIFICATION PRIOR TO THE START OF CONSTRUCTION WILL RESULT IN THE ISSUANCE OF A STOP CONSTRUCTION ORDER AND MAY BE CAUSE FOR LEGAL ACTION. NOTICE MAY BE FAXED TO (908) 788-0795 OR MAILED TO: HUNTERDON COUNTY SOIL CONSERVATION DISTRICT 687 PITTSTOWN ROAD FRENCHTOWN, NJ 08825 2. LAND DISTURBANCE AND CONSTRUCTION WORK START INCLUDES ANY DEMOLITION OR CLEARING THAT TAKES PLACE ON THE PROJECT SITE. APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED AT THE PROPOSED DEMOLITION AREAS. 3. THE PROJECT APPLICANT AND CONTRACTOR ARE TO BE AWARE THAT ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL MEASURES MAY BE REQUIRED BY THE SOIL CONSERVATION DISTRICT OR MUNICIPAL ENGINEER IF FIELD CONDITIONS OR UNFORESEEN SITUATIONS WARRANT THEM 4. THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT ENCOURAGES THE INSTALLATION AND STABILIZATION OF PERMANENT DETENTION OR RETENTION FACILITIES FROM THE START OF THE PROJECT. THIS IS PARTICULARLY IMPORTANT ON ACCOUNT OF THE STEEP TOPOGRAPHY AND SOILS OF HUNTERDON
- COUNTY. PRIORITY SHOULD TO BE SET ON CONSTRUCTION OF ANY THE DETENTION BASIN OR RETENTION BASIN FACILITY PRIOR TO ANY SIGNIFICANT AMOUNT OF LAND DISTURBANCE, SEDIMENT RISERS CAN BE USED ON A DETENTION BASIN AT ANY TIME AS LONG AS THEY MEET THE CRITERIA OF THE STATE SOIL EROSION AND SEDIMENT CONTROL STANDARDS. IF A SEDIMENT BASIN IS DESIGNED, AS EITHER WITHIN THE PERMANENT BASIN AREA OR AS A STAND-ALONE BASIN, THEY ARE TO BE DESIGN FULLY IN COMPLIANCE WITH THE STANDARDS AND ARE TO BE PROPERLY MAINTAINED DURING CONSTRUCTION. ALL DETENTION/RETENTION BASINS BE COMPLETED AND PERMANENTLY STABILIZED (ALONG WITH CONDUIT OUTLET PROTECTION AND LOW-FLOW CHANNEL) BEFORE ANY STORM DRAINAGE PIPING IS INSTALLED TO THE BASIN AND SAME PIPING IS FUNCTIONING. NO PAVING IS TO TAKE PLACE ON THE PROJECT SITE UNTIL ALL STORMWATER DETENTION/RETENTION FACILITIES ARE ADEOUATELY STABILIZED AS PER PLAN. FAILURE TO MAINTAIN A DETENTION, RETENTION, OR SEDIMENT FACILITY IN WORKING ORDER DURING CONSTRUCTION MAY BE GROUNDS FOR ISSUANCE OF A STOP CONSTRUCTION ORDER BY THE SOIL CONSERVATION DISTRICT
- 6. THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT DOES NOT SUPPORT NOR ENDORSE MASS EXCAVATION. THE AMOUNT OF SOIL DISTURBED AT ONE TIME, AND SUBJECT TO EROSION, IS TO BE KEPT TO A MINIMUM. IT IS THE POLICY OF THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT THAT LARGE DISTURBANCES OF SOIL EXPOSED AT ONE TIME ON A PROJECT WILL REOUIRE A DETAILED PLAN AND TIME-LINE FOR GETTING AREAS STABILIZED. THE STANDARD FOR SEDIMENT BARRIERS WILL BE USED FOR LIMITING LARGE AREAS OF EXCAVATION. IF EXCAVATIONS ARE PROPOSED THAT EXCEED THE SEDIMENT BARRIER STANDARD, THEN ADDITIONAL MEASURES ARE TO BE DESIGNED AND DETAILED AND A DETAILED SEQUENCE OF CONSTRUCTION BE SUBMITTED FOR RE-CERTIFICATION AND APPROVAL. AS A MINIMUM, SOILS EXPOSED FOR LONGER THAN 30 DAYS WILL REOURE TEMPORARY STABILIZATION FOLLOWING THE AGRONOMIC SPECIFICATIONS ON THE PLAN.
- 7. A COPY OF THE CERTIFIED SOIL EROSION AND SEDIMENT CONTROL PLAN IS TO BE KEPT ON THE PROJECT SITE DURING CONSTRUCTION AND AVAILABLE FOR REVIEW BY THE CONTRACTOR AND SOIL CONSERVATION DISTRICT INSPECTORS.
- 8. THE LAND DISTURBANCE IS TO PROCEED IN ACCORDANCE WITH THE APPROVED SEQUENCE OF CONSTRUCTION AND THE CERTIFIED PLAN. ALL REQUIRED SOIL EROSION AND SEDIMENT CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED AS OUTLINED IN THE PLAN
- 9. THE SOIL CONSERVATION DISTRICT IS TO BE NOTIFIED AND REPRESENTED AT A PRECONSTRUCTION CONFERENCE (USUALLY HELD AT THE MUNICIPAL ENGINEERS OFFICE) PRIOR TO THE START OF CONSTRUCTION OR ANY LAND DISTURBANCE.
- 10. ALL DISTURBED AREAS THAT ARE NOT BEING GRADED, NOT UNDER ACTIVE CONSTRUCTION, OR NOT SCHEDULED TO BE PERMANENTLY SEEDED WITHIN 30 DAYS MUST BE TEMPORARILY STABILIZED AS PER SPECIFICATIONS BELOW.
- 11. ALL EXPOSED AREAS WHICH ARE TO BE PERMANENTLY VEGETATED, ARE TO BE SEEDED AND MULCHED WITHIN 10 DAYS OF FINAL GRADING.
- 12. STRAW MULCH (HAY MULCH MAY BE SUBSTITUTED IF APPROVED BY THE DISTRICT) IS TO BE APPLIED TO ALL SEEDINGS AT A RATE OF 1-1/2 TO 2 TONS PER ACRE (APPROX. 100 TO 130 BALES PER ACRE).
- 13. MULCH ANCHORING IS REQUIRED AFTER MULCHING TO MINIMIZE LOSS BY WIND OR WATER. THIS IS TO BE DONE USING ONE OF THE METHODS (CRIMPING LIQUID MULCH BINDERS, NETTINGS, ETC.) IN THE "STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY". 14. EXISTING WEEDY AND POORLY-VEGETATED AREAS WITH LESS THAN 80 PERCENT PERENNIAL GRASS COVER MUST RECEIVE PERMANENT STABILIZATION AS
- PER THESE SPECIFICATIONS. 15. ALL BAGS NEED TO BE SAVED FOR LIME, FERTILIZER, SEED, AND LIQUID MULCH BINDER (IF MULCH ANCHORING METHOD). SUCH PROOFS NEED TO BE
- SUBMITTED TO THE DISTRICT INSPECTOR FOR VERIFICATION OF MATERIALS AND QUANTITIES USED FOR ALL SEEDINGS. 16. AN ADDITIONAL FEE PER INSPECTION (AS PER THE CURRENT HUNTERDON COUNTY SOIL CONSERVATION DISTRICT FEE SCHEDULE AT THE TIME OF INSPECTION) WILL BE ASSESSED ON THOSE SITES WHERE ADDITIONAL INSPECTIONS ARE NECESSITATED AS A RESULT OF NON-COMPLIANCE WITH THE APPROVED PLAN. THIS INCLUDES ADDITIONAL INSPECTIONS PERFORMED AFTER THE FAILURE OF AN INITIAL REPORT OF COMPLIANCE INSPECTION, THE ENTIRE PROJECT SITE IS INSPECTED AT THE TIME OF A REOUEST FOR REPORT OF COMPLIANCE.
- 17. SOILS IN HUNTERDON COUNTY REQUIRE THAT ALL STONE TRACKING PADS (STABILIZED CONSTRUCTION ENTRANCE) BE INSTALLED AT A MINIMUM OF 100 FT. IN LENGTH FOR ROADWAY GRADES OF 0% TO 2% AND 200 FT. FOR ACCESS GRADES GREATER THAN 2%. THIS REQUIREMENT IS THE SAME, REGARDLESS IF MAIN PROJECT ENTRANCE OR INDIVIDUAL DWELLING LOT. STONE TRACKING PADS OR OTHER MEASURES APPROVED BY THE SOIL CONSERVATION DISTRICT ARE TO BE INSTALLED AT ALL CONSTRUCTION ACCESSES TO PAVEMENT. SEE DETAIL PLAN SHEET
- THE CONSTRUCTION ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO ROADWAYS. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDITIONAL LENGTH AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO ROADWAYS (PUBLIC OR PRIVATE) OR OTHER IMPERVIOUS SURFACES MUST BE REMOVED IMMEDIATELY.
- WHERE ACCUMULATING OF DUST/SEDIMENT IS INADEQUATELY CLEANED OR REMOVED BY CONVENTIONAL METHODS, A POWER BROOM OR STREET SWEEPER WILL BE REQUIRED TO CLEAN PAVED OR IMPERVIOUS SURFACES. ALL OTHER ACCESS POINTS, WHICH ARE NOT STABILIZED, SHALL BE ADEQUATELY BLOCKED OFF
- 18. CONDUIT OUTLET PROTECTION (RIP-RAP APRONS OR SCOUR HOLES) MUST BE DESIGNED AND INSTALLED AT ALL PIPE OUTLETS AS PER THE CERTIFIED PLANS AND STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY. CONDUIT OUTLET PROTECTION MUST BE INSTALLED IMMEDIATELY FOLLOWING PIPE INSTALLATION AND PRIOR TO ANY PIPE FLOW. CONDUCT OUTLET PROTECTION MUST BE MAINTAINED AS PER DESIGN UNTIL THE COMPLETION OF THE PROJECT AND ISSUE OF FINAL REPORT OF COMPLIANCE. SEE CONDUIT OUTLET PROTECTION DETAIL AND SPECIFICATION TABLE ON PLAN SHEET
- 19. ALL STORMWATER INLET PROTECTION NEEDS TO BE MAINTAINED PERIODICALLY WITH FRESH HAYBALES OR CLEAN STONE BERMS (STONE SIZED 1 1/2"-2 1/2") OR APPROVED METHOD TO COMPLETELY ENCIRCLE, BUT NOT BLOCK THE INLETS. SEE DETAIL ON PLAN SHEET ____. INSPECTIONS OF STORMWATER INLET PROTECTION SHALL BE FREQUENT. MAINTENANCE, REPAIR, AND REPLACEMENT SHALL BE MADE PROMPTLY, AS
- NEEDED. INLET PROTECTION NEEDS TO BE MAINTAINED UNTIL ALL AREAS OF THE SITE, OR AS A MINIMUM THE AREA DRAINING TO THE INLET, ARE PERMANENTLY STABILIZED AND APPROVED BY SOIL CONSERVATION DISTRICT INSPECTORS.
- 20. DUST CONTROL MEASURES ARE TO BE USED DURING ALL PHASES OF CONSTRUCTION OF THE PROJECT. SEE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY PAGES 16-1 AND 16-2. SEE DUST CONTROL MATERIALS TABLE ON PLAN SHEET 21. ALL TREES THAT ARE TO BE PROTECTED FROM ENVIRONMENTAL AND MECHANICAL INJURY DURING CONSTRUCTION ARE TO BE ADEQUATELY MARKED IN
- FENCED-OFF PRIOR TO CONSTRUCTION AND MAINTAINED DURING CONSTRUCTION. FOR FURTHER INFORMATION SEE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY PAGES 9-1 THROUGH 9-7. SEE PROPER TREE PROTECTION DETAIL ON PLAN SHEET
- 22. DEWATERING METHODS ARE TO BE FOLLOWED TO PROPERLY REMOVE SUSPENDED SEDIMENTS IN WATER FROM EXCAVATIONS AND/OR TRENCHES PRIOR TO DISCHARGE TO DOWNSTREAM AREAS AND/OR WATERCOURSES. THESE METHODS ARE TO FOLLOW THOSE FOUND IN THE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY, PAGES 14-1TO 14-7.
- 23. ON SUBDIVISION PLANS, INDIVIDUAL LOTS ON STEEP SLOPES (GREATER THAN 10%) OR IN CLOSE PROXIMITY TO A DRAINAGEWAY, REQUIRE AN INDIVIDUAL

SOIL EROSION AND SEDIMENT CONTROL/GRADING TO BE SUBMITTED AND CERTIFIED PRIOR TO OBTAINING A BUILDING PERMIT AND BEFORE ANY LAND DISTURBANCE ON THAT LOT. THESE INDIVIDUAL LOT PLANS ARE CONSIDERED MINOR REVISIONS TO A CERTIFIED PLAN AND WILL BE SUBJECT TO A RESUBMISSION FEE FOR REVIEW AND CERTIFICATION AS PER THE CURRENT HUNTERDON COUNTY SOIL CONSERVATION DISTRICT FEE SCHEDULE AT THE TIME OF SUBMISSION THE LOTS REQUIRING INDIVIDUAL LOT PLANS FOR THIS PROJECT ARE

24. AS PER THE TRAFFIC CONTROL STANDARD IN THE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY (PAGE 33-1) STEEP BANKS, WETLAND BUFFERS, WATERWAYS, AND OTHER SENSITIVE AREAS ARE TO BE AVOIDED BY CONSTRUCTION TRAFFIC. WETLAND BUFFER AND WETLAND AREAS ARE TO BE ADEOUATELY MARKED IN FIELD PRIOR TO CONSTRUCTION AND MAINTAINED DURING CONSTRUCTION.

25. ANY FORMER AGRICULTURAL CROP FIELDS THAT ARE EITHER IN CROPS, CROP RESIDUE, OR ANNUAL WEED COVER ARE TO BE STABILIZED FOLLOWING THE AGRONOMIC SPECIFICATIONS FOR HUNTERDON COUNTY. THIS IS TO BE EITHER A COVER CROP FROM THE PERIOD OF LAST HARVEST AND CONSTRUCTION START-UP OR TEMPORARY STABILIZATION THROUGH SEEDING AND MULCHING AREAS THAT ARE NOT GOING TO BE EITHER BUILT ON OR CONTINUED TO BE FARMED ARE TO RECEIVE PERMANENT STABILIZATION.

26. IF EXCESS FILL OR ANY OTHER MATERIAL IS TO BE REMOVED FROM THE SITE, THE PROJECT OWNER/APPLICANT SHALL BE RESPONSIBLE FOR ITS PROPER DISPOSAL AND WILL NOTIFY THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT AS TO THE PLANNED DISPOSAL SITE LOCATION. IF APPLICABLE, A SOIL EROSION AND SEDIMENT CONTROL PLAN MUST BE SUBMITTED TO REVIEWED AND CERTIFIED BY THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT PRIOR TO ANY MATERIAL REMOVAL FROM THE PROJECT SITE. REMOVAL OF ANY SOIL MATERIAL FROM THE PROJECT SITE WITHOUT WRITTEN AUTHORIZATION FROM THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT IS A VIOLATION OF THE STATE SOIL EROSION AND SEDIMENT CONTROL

27. STOCKPILING OF FINES (SAND, QUARRY-PROCESS-BLEND, ETC.) IS NOT ALLOWED ON PAVED SURFACES OF THE PROJECT SITE.

28. ANY GABION BASKETS USED ON THE PROJECT ARE TO BE COATED WITH PLASTIC OR PVC AND FILLED WITH 4"-7" ANGULAR ROCK. THE GABION THICKNESS IS TO BE AT LEAST THE CALCULATED STONE D50 SIZE OF A REGULAR RIP-RAP APRON. FILTER FABRIC IS TO BE INSTALLED BETWEEN THE SUBGRADE AND THE

29. THE LIMITS OF DISTURBANCE SHOWN ON THE PLANS ARE NOT TO BE EXCEEDED UNLESS AUTHORIZED BY THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT AND A REVISED PLAN SUBMITTED FOR CERTIFICATION.

30. ALL DISTURBED ROADSIDE AREAS NEED TO BE TOPSOILED, FINAL-GRADED, LIMED, FERTILIZED, SEEDED, MULCHED, AND MULCH-ANCHORED (FOLLOWING DISTRICT AGRONOMIC SPECIFICATIONS FOR PERMANENT SEEDING)FOR A MINIMUM DISTANCE APPROVED BY THE DISTRICT BACK FROM THE CURB-LINE PRIOR TO APPROVAL OF PERMANENT IMPROVEMENT.

31. THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT DOES NOT RECOMMEND THE USE OF RIP-RAP D50 SIZES SMALLER THAN 6" FOR APRONS OR SCOUR HOLES SINCE SMALLER STONE SIZES (3" OR 4") TEND TO WASH/ERODE UNDER HIGH INTENSITY RAIN STORMS. THE HUNTERDON DISTRICT RECOMMENDS THAT THE SMALLEST D50 STONE SIZE BE SPECIFIED AS 6" WITH THICKNESS SPECIFIED AS 12" WITH FILTER FABRIC OR 18" WITHOUT FABRIC

32. TEMPORARY DIVERSIONS TO DIRECT WATER OFF OF A GRADED RIGHT-OF-WAY ONTO A STABLE AREA ARE NEEDED DURING CONSTRUCTION. FOR FURTHER INFORMATION REFER TO THE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY, (PAGE 15.3 ITEM 2 AND FIGURE 15-4) FOR THE REQUIRED DIMENSIONS AND SPACING. SEE DETAIL AND SPACING ON PLAN SHEET

33. A SEDIMENT BARRIER MUST BE INSTALLED ABOVE ANY DETENTION/RETENTION BASINS (BETWEEN THE ROADWAY/BUILDING CONSTRUCTION AND DETENTION BASIN). THIS IS TO PROTECT THE DETENTION BASIN NEWLY GRADED/SEEDED AREAS WHILE THE OTHER CONSTRUCTION IS BEING COMPLETED AND ALL UPSTREAM AREAS ARE PERMANENTLY STABILIZED

34 HYDROSEEDING/HYDROMIII CHING ARE NOT RECOMMENDED PRACTICES IN HUNTERDON COUNTY DUE TO THE HIGH FAILURE RATE OF SEEDINGS. STEEP TOPOGRAPHY, POOR SEED-TO-SOIL CONTACT AND POOR GROUND SURFACE COVERAGE. ALL SEED MUST BE INCORPORATED INTO THE SOIL. HYDROSEEDING EQUIPMENT MAY BE USED IN CONJUNCTION WITH STRAW/HAY MULCH FOR THE PURPOSE OF ANCHORING THE MULCH WITH LIQUID MULCH BINDERS. 35. IF SUBSURFACE WATER PROBLEMS ARE DISCOVERED DURING CONSTRUCTION, THEY WILL BE RECTIFIED FOLLOWING THE STANDARDS FOR SOIL EROSION

AND SEDIMENT CONTROL IN NEW JERSEY (SUBSURFACE DRAINAGE, PAGE 32-1 THROUGH 32-4) 36. ALL DEVELOPMENT ROADWAYS ARE TO BE KEPT SCRAPED/SWEPT TO REMOVE SEDIMENT ACCUMULATIONS ALONG CURBS AND AROUND STORMWATER

37. THE MAXIMUM ALLOWABLE VEGETATED SLOPE IS 2:1. SLOPES IN EXCESS OF 3:1 (BETWEEN 2:1 AND 3:1) REQUIRE TEMPORARY EROSION CONTROL MATTING, SUCH AS EXCELSIOR "CURLEX" OR EQUIVALENT, FOR STABILIZATION. THE MATTING IS TO BE PROPERLY INSTALLED WITH SPECIFIED OVERLAP, CHECK SLOTS, ANCHORING SPACING, AND ANCHORING DEVICE TYPE, GAUGE, AND SIZE.

38. ALL DISTURBED AREAS THAT ARE NOT BEING GRADED, NOT UNDER ACTIVE CONSTRUCTION, OR NOT SCHEDULED TO BE PERMANENTLY SEEDED WITHIN 30 DAYS MUST BE TEMPORARILY STABILIZED AS PER THE AGRONOMIC SPECIFICATIONS.

39. A REPORT OF COMPLIANCE FROM THE SOIL CONSERVATION DISTRICT IS REQUIRED FOR EACH DWELLING LOT PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY ON THAT LOT. FAILURE TO MAINTAIN OR COMPLY WITH THE SOIL EROSION AND SEDIMENT CONTROL PLAN FOR THE PROJECT WILL BE CAUSE FOR COMPLIANCE FAILURE ON AN INDIVIDUAL LOT

40. IT IS THE OWNER/APPLICANTS RESPONSIBILITY TO NOTIFY THE DISTRICT OF ALL PROPERTY CONVEYANCES AND SALE OF INDIVIDUAL LOTS ON A PROJECT. SOIL EROSION AND SEDIMENT CONTROL PLAN APPLICATIONS ARE TO BE FILED BY ANY NEW OWNERS ON PROJECTS/LOTS WHERE CONSTRUCTION ACTIVITIES ARE TO STILL TAKE PLACE.

41. PURSUANT TO THE NEW JERSEY SOIL EROSION AND SEDIMENT CONTROL ACT, CHAPTER 251, P.L. 1975, THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT HAS REVIEWED THE PLANS FOR THIS PROJECT AND CERTIFIES THE SOIL EROSION AND SEDIMENT CONTROL PLAN. THE APPROVAL OF THE SOIL FROSION AND SEDIMENT CONTROL PLANS BY THE SOIL CONSERVATION DISTRICT IS LIMITED TO THE SOIL EROSION, SEDIMENTATION, AND RELATED STORMWATER MANAGEMENT CONTROLS SPECIFIED IN THIS PLAN. IT IS NOT AUTHORIZATION TO ENGAGE IN THE PROPOSED LAND USE UNLESS THE MUNICIPALITY OR OTHER CONTROLLING AGENCY HAS PREVIOUSLY APPROVED SUCH USE

42. PLANS SUBMITTED TO THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT MUST BE CONSISTENT WITH PLANS ANY PLANS SUBMITTED TO A REGULATORY AGENCY SUCH AS NJDEP, MUNICIPALITY, ETC. ANY REVISIONS REQUIRED BY ANY REVIEWING AUTHORITY WOULD REQUIRE A RESUBMISSION TO THE DISTRICT FOR REVIEW.

3. ALL REVISIONS AND MUNICIPAL RENEWALS OF THIS PROJECT WILL REQUIRE RESUBMISSION AND APPROVAL BY THE SOIL CONSERVATION DISTRICT 44. REPORT OF COMPLIANCE: A REPORT OF COMPLIANCE APPROVING PERMANENT STABILIZATION MEASURES (OR A SOIL EROSION AND SEDIMENT CONTROL COMPLETION BOND AGREEMENT WITH TEMPORARY STABILIZATION FOR THE WINTER SEASON) IS TO BE ISSUED BY THE SOIL CONSERVATION DISTRICT ON ALL PROJECTS AT THEIR COMPLETION. BEFORE ANY CERTIFICATE OF OCCUPANCY (PERMANENT OR TEMPORARY) CAN BE GRANTED BY THE MUNICIPALITY OR STATE, A WRITTEN REPORT OF COMPLIANCE MUST BE ISSUED BY THE SOIL CONSERVATION DISTRICT.

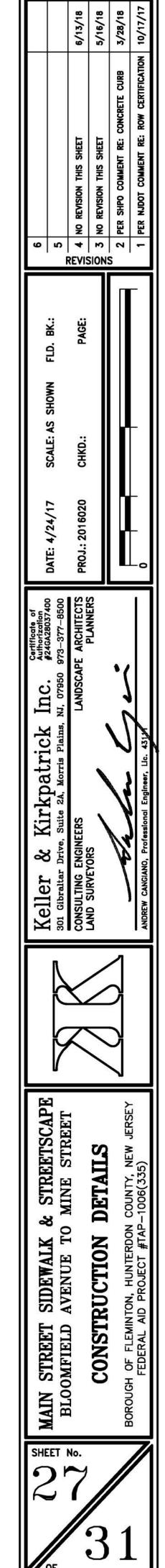
45. PURSUANT TO AUTHORITY GRANTED BY N.J.S.A. 4:24 47, THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT PERIODICALLY INSPECTS THE PROJECT SITE FOR COMPLIANCE WITH THE CERTIFIED SOIL EROSION AND SEDIMENT CONTROL PLANS AND THE STATE SOIL EROSION AND SEDIMENT CONTROL ACT. FAILURE TO COMPLY WITH THE PLANS AND THE ACT MAY BE CAUSE FOR COURT ACTION AND PENALTIES, PURSUANT TO N.J.S.A. 4:24 53. THE MAXIMUM STATUTORY PENALTY PROVIDED BY LAW FOR VIOLATIONS OF THE SOIL EROSION AND SEDIMENT CONTROL ACT IS A FINE OF UP TO \$3,000 EACH DAY AND AN INJUNCTIVE ORDER OF THE SUPERIOR COURT

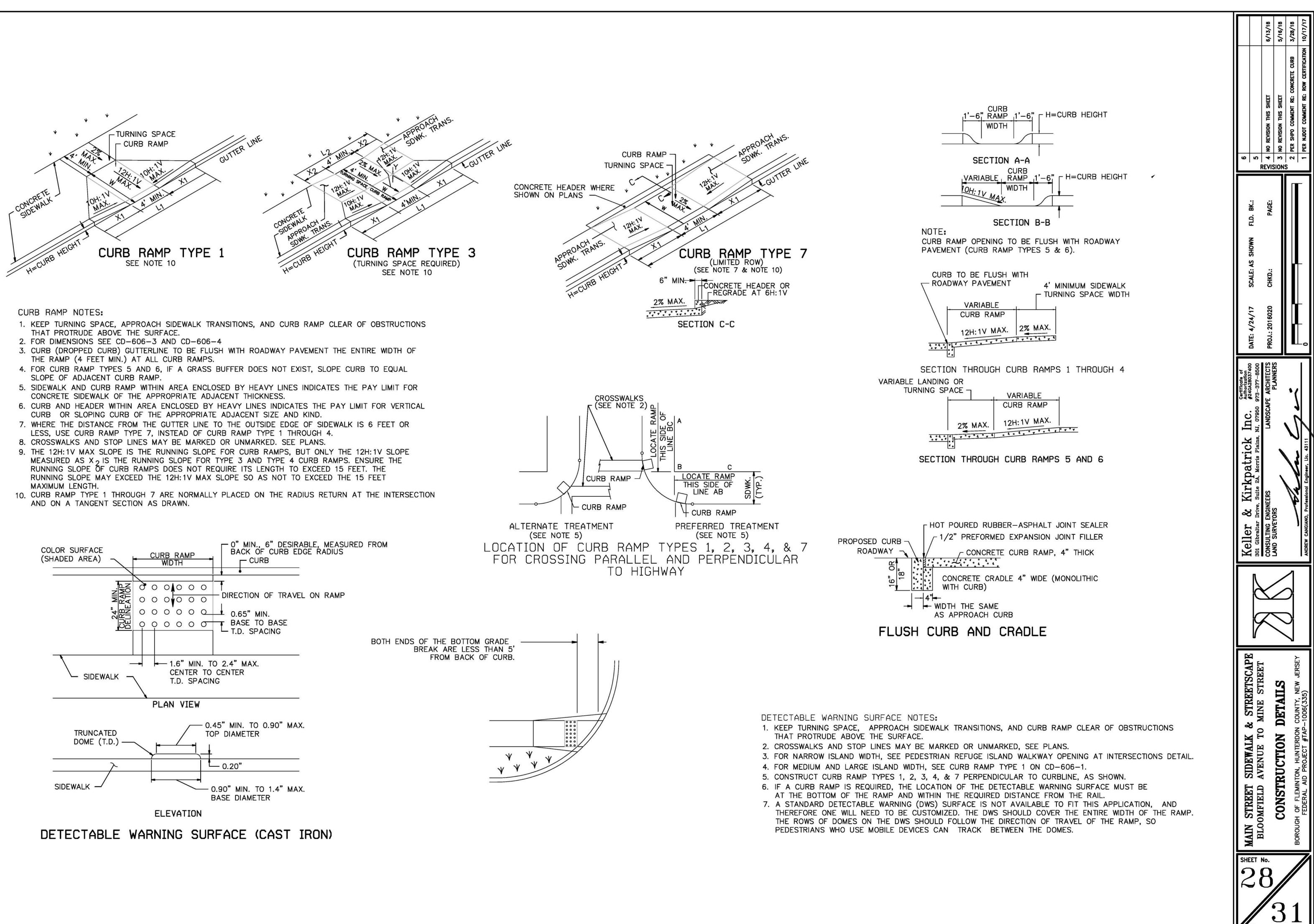
46. IT IS POLICY OF THE HUNTERDON COUNTY SOIL CONSERVATION DISTRICT TO PERIODICALLY EVALUATE ALL PROJECTS TO DETERMINE IF THE COSTS FOR REVIEW AND INSPECTION EXCEED THE PAID FEES. PRIOR TO THE FEES BEING EXCEEDED AN ADDITIONAL FEE WILL BE ASSESSED. THIS FEE WILL BE BASED ON THE INCOMPLETE PORTION(S) OF THE PROJECT, REGARDLESS IF PRESENTLY UNDER CONSTRUCTION OR NOT, AS PER THE CURRENT HUNTERDON COUNTY SOIL CONSERVATION DISTRICT FEE SCHEDULE AT THE TIME OF EVALUATION

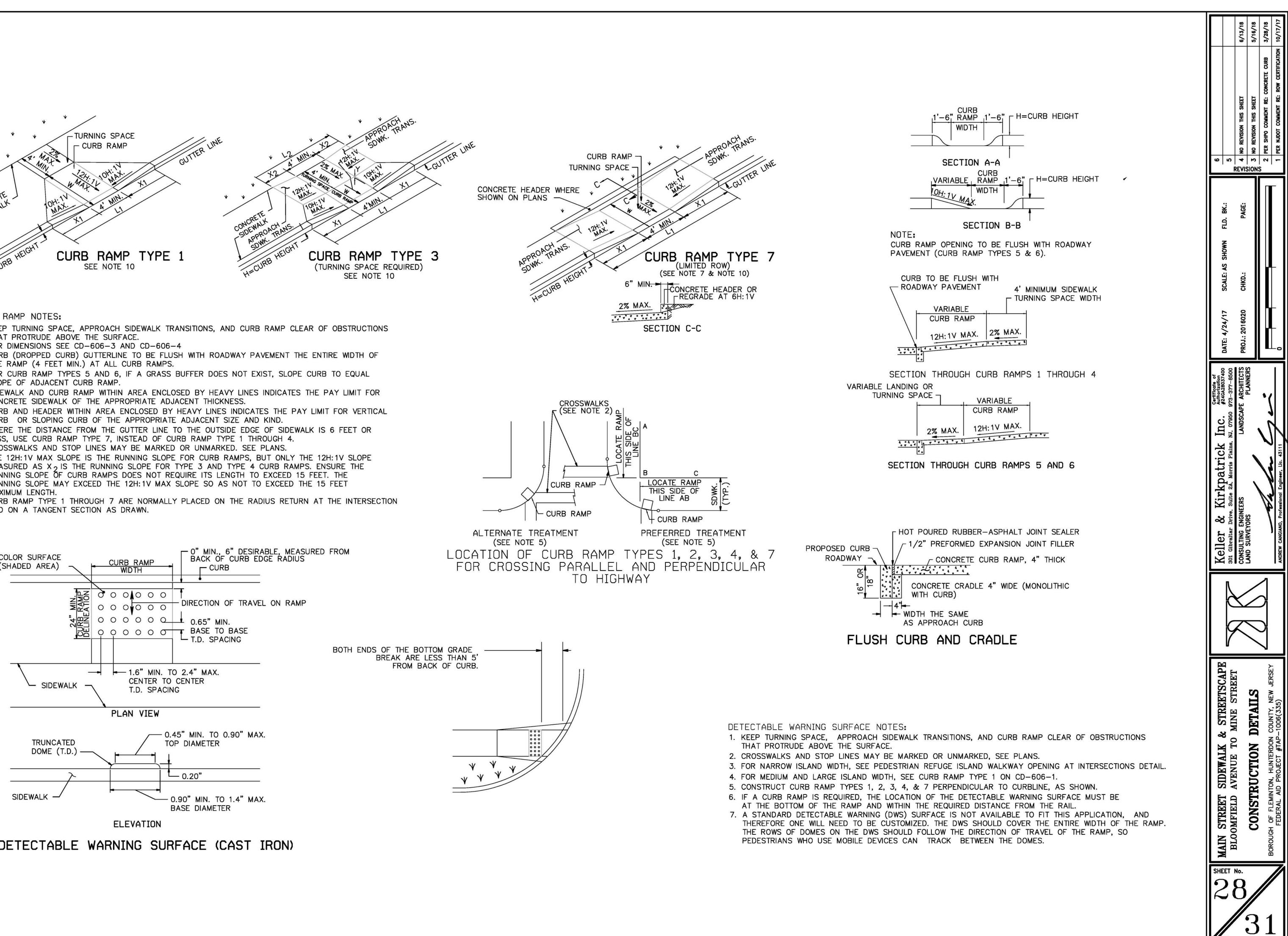
47. SOIL COMPACTION: AREAS OF TRAVEL WITHIN A PROJECT SITE AND/OR STAGING AND PARKING AREAS MAY HAVE SOILS COMPACTED DURING THE COURSE OF PROJECT CONSTRUCTION. ALL SOIL COMPACTION IS TO BE CORRECTED PRIOR TO ANY PERMANENT STABILIZATION AND COMPLETION OF PROJECT. THE TOPSOILING STANDARD (NJ SE&SC STANDARDS PAGE 8-2) STAtes that where there is compaction, the surface is to be scarified 6" to 12" prior to applying topsoil for permanent stabilization. The Soil Conservation District will be inspecting for this to be employed prior to any permanent stabilization and prior to issue of any Report of Compliance. Where topsoil is not being stripped during construction, pre and post compaction test may be used to verify that construction traffic has not caused a soil compaction problem to the site.

SEQUENCE OF CONSTRUCTION

AC	ΤΙΜΤΥ	APPROX. DURATION (DAYS)
1.	NOTIFY DISTRICT 48 HOURS BEFORE START OF CONSTRUCTION.	N/A
2.	INSTALL INLET FILTERS AND EROSION CONTROL MEASURES AROUND WORK AREA.	1
3.	CONSTRUCT SIDEWALK AND STREETSCAPE IMPROVEMENTS	30
4.	STABILIZE AREAS NOT SUBJECT TO CONSTRUCTION TRAFFIC THAT WILL BE LEFT EXPOSED FOR MORE THAN 30 DAYS.	ON GOING
5.	RESTORE ALL ACCESS AREAS AND ITEMS TEMPORARILY MOVED OR DAMAGED DURING CONSTRUCTION.	5
6.	BEGIN PERMANENT STABILIZATION METHODS (TOPSOIL, FERTILIZER, SEED) THROUGHOUT THE WORK SITE.	2
7.	REMOVE ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.	1
8.	NOTIFY DISTRICT OF COMPLETION OF WORK.	N/A







GENERAL NOTES:

- ADVANCE WARNING SIGNS, DISTANCES, AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE ENGINEER, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.
- THE APPROXIMATE LOCATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS. THESE LOCATIONS MAY BE MODIFIED TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION. ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.
- PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE IN PLACE.
- RAMPS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN SHALL BE PROVIDED WITH AT LEAST ONE W20-IF SIGN (ROAD WORK AHEAD) AS A MINIMUM.
- ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS AND/OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN SHALL BE COVERED, REMOVED OR RELOCATED AS DIRECTED BY THE ENGINEER.
- CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON EITHER THE EXISTING, TEMPORARY, OR PROPOSED TRAFFIC SIGNAL SYSTEMS SHALL BE BAGGED OR COVERED.
- MAINTENANCE AND PROTECTION OF TRAFFIC SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES - PART VI "STANDARDS AND GUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS". UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS, AND SHALL BE APPROVED BY THE ENGINEER.
- CONSTRUCTION SIGN W99-2 (GIVE US A BRAKE) SHALL BE LOCATED 200 FEET IN 8. ADVANCE OF PROJECT LIMITS.
- A W1-6 (ARROW) SIGN MOUNTED ON A BREAKAWAY BARRICADE AND CENTERED ON THE CLOSED WIDTH SHALL BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.
- CONSTRUCTION SIGNS R11-4 (ROAD CLOSED TO THRU TRAFFIC) SHALL BE PLACED 10. AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BECAUSE OF CONSRUCTION.
- CONSTRUCTION SIGNS W8-9A (SYMBOL FOR UNEVEN PAVEMENT) AND W8-14A (GROOVED PAVEMENT) SHALL BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST. THE PLACEMENT OF THESE SIGNS SHALL BE AS DIRECTED BY THE ENGINEER.
- MOVING WORK AREAS IN A PERMANENT LANE CLOSURE REQUIRE A TRAILER MOUNTED ILLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER. THE TRUCK MOUNTED CRASH CUSHION SHALL MOVE WITH THE WORK AREAS TO KEEP A 60 FOOT MINIMUM AND 150 FOOT MAXIMUM BUFFER IN ADVANCE OF EACH WORK AREA.
- THE CONTRACTOR SHALL SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION 13. VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES. THE PLAN SHALL BE SUBMITTED TO THE ENGINEER IN ACCORDANCE WITH SECTION 617 OF THE STANDARD SPECIFICATIONS.
- TRAFFIC SAFETY SERVICES SHALL BE USED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL, SECTION 617.
- 15. ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY SHALL BE BACKFILLED AND PLACED ON A MINIMUM 6H : 1V SLOPE PRIOR TO THE END OF EACH WORK DAY. OTHER EXCAVATED AREAS WITHIN THE CLEAR ZONE ARE TO BE EITHER BACKFILLED OR A PRECAST CONCRETE CURB CONSTRUCTION BARRIER SET TEMPORARILY IN PLACE TO SHIELD VEHICULAR AND PEDESTRIAN TRAFFIC.
- WHERE REQUIRED. THE CONTRACTOR SHALL MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE ENGINEER.
- BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES SHALL 17. BE TRANSITIONED ON A MINIMUM 20H : 1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.
- 18. THE PLACEMENT AND OR RELOCATION OF PRECAST CONCRETE CURB, CONSTRUCTION BARRIER SHALL BE DONE DURING APPROVED OFF-PEAK HOURS WHEN TRAFFIC MAY BE REDUCED TO ONE LANE IN EACH DIRECTION.
- 19. CONSTRUCTION ZONE SPEED LIMIT WILL BE DETERMINED BY TRAFFIC SIGNAL & SAFETY ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED BY THE RESIDENT ENGINEER.
- 20. THE SPEED LIMIT, R2-1 (BLACK ON WHITE) SIGN SHALL BE LOCATED THROUGH WORK AREAS AS DIRECTED BY TRAFFIC SIGNAL & SAFETY ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE.
- 21. THE REDUCED SPEED AHEAD SIGN, R2-5A(S) (BLACK ON WHITE) SHALL BE LOCATED IN ADVANCE OF SPEED LIMIT R2-1 SIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.
- 22. TRAFFIC FINES DOUBLED IN WORK AREA R(NJ)5-17(S), 4 FEET BY 2.5 FEET SIGN SHALL BE LOCATED 500 FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS. THIS SIGN SHALL ALSO BE USED ON PROJECTS REQUIRING MOVING OPERATIONS IN WHICH CASE THE SIGN SHALL BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.
- 23. THE FINAL BITUMINOUS CONCRETE SURFACE PAVEMENT SHALL NOT BE CONSTRUCTED UNTIL THE FINAL STAGE OF THE PROJECT. MANHOLES AND INLETS SHALL BE SET TO FINISHED GRADE AND TEMPORARY PAVEMENT RAMPS ARE TO BE CONSTRUCTED AROUND THEM WITH A MINIMUM 20H : 1V SLOPE IN ALL DIRECTIONS USING HOT MIX ASPHALT PAVEMENT. THIS TEMPORARY MATERIAL WILL BE REMOVED IMMEDIATELY PRIOR TO PLACING THE SURFACE COURSE.

- CLOSURES.
- ENGINEER.
- 26. TERMS FOLLOWS:
 - SWEEPING, LANE SHIFTS, OR ALTERNATING TRAFFIC. THIS APPLIES EVEN WHEN DETOURS ARE PROVIDED.

TEMPORARY LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH IS ROUTINELY SET UP AND REMOVED ON A DAILY BASIS.

PERMANENT LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.

27. ADVANCE NOTICES

FOR THE INITIAL START OF WORK THAT REQUIRES "Impacts to Normal Traffic Flow", THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IN WRITING. ON THE ADVANCE FORM TO-103 PROVIDED BY THE DEPARTMENT, OF THE PROPOSED DATE. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS. BUT NOT MORE THAN SIXTY CALENDAR DAYS. BEFORE THE PROPOSED DATE. START OF WORK THAT IMPACTS NORMAL TRAFFIC FLOW WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RESIDENT ENGINEER. THE PROPOSED DATE SEVEN (AND/OR FOURTEEN) CALENDAR DAYS BEFORE STARTING THE ESTABLISHMENT OF THE TRAFFIC CONTROL MEASURES FOR THE TRAFFIC IMPACT. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESIDENT ENGINEER IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.

FOR A "PERMANENT LANE CLOSURE", THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IN WRITING, ON ADVANCE FORM TO-103, OF THE PROPOSED DATE A NEW TRAFFIC PATTERN WILL BE ESTABLISHED. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS. BUT NOT MORE THAN SIXTY CALENDAR DAYS. IN ADVANCE OF THE PROPOSED DATE. START OF A NEW TRAFFIC PATTERN WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RESIDENT ENGINEER, THE PROPOSED DATE OF THE NEW TRAFFIC PATTERN SEVEN (AND/OR FOURTEEN) DAYS BEFORE STARTING TRAFFIC CONTROL MEASURES FOR THE ESTABLISHMENT OF THE NEW PATTERN. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESIDENT ENGINEER IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.

STARTING THE ESTABLISHMENT OF A NEW PERMANENT TRAFFIC PATTERN SHALL BEGIN NO EARLIER THAN 11:00 PM FRIDAY AND SHALL BE COMPLETED AND READY FOR OPERATIONS BY 6:00 PM THE FOLLOWING SUNDAY, THE ESTABLISHMENT SHALL BE COMPLETED IN ACCORDANCE WITH THE LANE CLOSURE HOURS SPECIFIED IN THE CONTRACT.

ADVANCE NOTICES SENT PRIOR TO THE PRE-CONSTRUCTION MEETING SHALL BE ADDRESSED TO THE CONTACT PERSON AS SPECIFIED IN SUBSECTION 101.04 OF THE 28. PROGRESS NOTICES

ALL "IMPACTS TO NORMAL TRAFFIC FLOW" SCHEDULED FOR THE SEVEN DAY PERIOD STARTING ON THE FOLLOWING MONDAY SHALL BE SUBMITTED TO THE RESIDENT ENGINEER BY 9:00 AM OF EACH FRIDAY ON WEEKLY FORM TO-101 PROVIDED BY THE DEPARTMENT.

EACH DAY OF "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE RESIDENT ENGINEER BY 9:00 AM THE DAY IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO-102 PROVIDED BY THE DEPARTMENT.

"TEMPORARY LANE CLOSURES" FOR WEEKENDS SHALL BE SUBMITTED TO THE RESIDENT ENGINEER BY 9:00 AM ON THE IMMEDIATELY PRECEDING FRIDAY ON THE DAILY FORM TO-102 PROVIDED BY THE DEPARTMENT.

29. CHANGES TO THE SCHEDULED CLOSURES

REQUEST FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS SHALL BE SUBMITTED IN WRITING TO THE RESIDENT ENGINEER AS FOLLOWS:

CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE RESIDENT ENGINEER AT LEAST EIGHT CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.

OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" SHALL BE SUBMITTED TO THE RESIDENT ENGINEER AT LEAST FORTY CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.

24. TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. SHALL BE PLACED AS SHOWN ON PLANS. SIGNS SHALL NOT BE PLACED WITHOUT ACTUAL LANE CLOSURES AND SHALL BE IMMEDIATELY REMOVED UPON REMOVAL OF THE

25. CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE

WHEN THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING SHALL BE AS

IMPACTS TO NORMAL TRAFFIC FLOW - WORK THAT REQUIRES A PORTION OF THE PAVES ROADWAY BEING BLOCKED OR CLOSED WITH SAFETY DEVICES OR VEHICLES. INCLUDING. BUT NOT LIMITED TO, FULL OR PARTIAL LANE CLOSURES, FULL OR PARTIAL RAMP CLOSURES, SHOULDER CLOSURES, MOVING OPERATIONS SUCH AS TRAFFIC STRIPING OR

LEGEND

LEFT

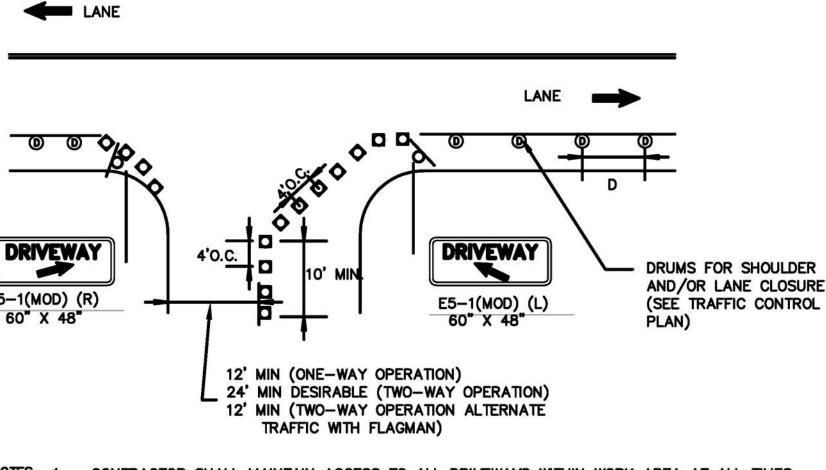
LEFT

	BREAKAWAY BARRICADES	REGULATORY APPROACH SPEED OF		NG OF CHANNELIZI				
1	BREAKAWAY BARRICADES WITH SIGN	TRAFFIC	DESIR	ABLE	MINIMUM			
•	CONSTRUCTION SIGNS		RURAL	URBAN	RURAL AND URBAN			
I	DRUMS	MILES/HOUR	FEET	FEET	FEET			
0	DRUMS	25	375	525	150			
\diamond	CONE	30	450	625	200			
	PRECAST CONCRETE CURB CONSTRUCTION BARRIER (TYPE SPECIFIED)	40 45	525 600	725 825	250			
	FRECAST CONCRETE CORB CONSTRUCTION BARRIER (TTPE SPECIFIED)	50	675	925	325 400			
4	DIRECTION OF TRAFFIC FLOW	55	750	1025	475			
		60	875	1150	550			
	FLAGGER	65	1000	1275	725			
0 0	ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING CAUTION MODE							
	ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING ARROW	RECOMMENDE FOR REGULATORY	RECOMMENDED SPACING ALONG TANGENTS					
T RIGHT BOTH	PATTERN (Left, Right, Both) TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING CAUTION MODE	APPROACH SPEED OF TRAFFIC IN MILES/HOUR 25 20:	WIDTH 10' 11'	MAXIMUM DEVICE (B) SPACING DTHS ALONG TAPERS IN FEET 12' 125 25	MAXIMUM DEVICE (D) SPACING ALONG TANGENTS IN FEET 50			
I IIIIIIIIIIIIIIIIIIIIIIIIIIIIIIIIIIII	TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING ARROW PATTERN (Left, Right, Both)	30 25: 35 20.5 40 27.5 45 45: 50 50:	1 130 165 :1 205 225 : :1 270 295 : 1 450 485 : 1 500 550	180 30 245 35 320 40 540 45 600 50	60 70 80 90 100			
888000	TEMPORARY CRASH CUSHION, INERTIAL BARRIER SYSTEM NOTE:	55 55:		660 55 C	120 D FOR TAPERS (B) IN THE			
\bigtriangleup	TEMPORARY CRASH CUSHION, (all other approved)	ABOVE TABLE.	ACING ALONG CORVES S	DRALL DE AS DEFINE	D FOR TAPERS (D) IN THE			
	BUFFER ZONE	GENERAL NOTES	FOR PLACEME	NT OF TRAFFI	C CONTROL DEVICES			
$\boxtimes \boxtimes$	WORK AREA	 AVOIDANCE MANEUVER IS FOR A SPEED, PATH, AND/OR DIRECTION CHANGE PRIOR TO BEGINNING OF CHANNELIZING TAPERS. RECOMMENDED DISTANCES BETWEEN TWO SEPARATE LANE CLOSURES SHALL BE DOUBL 						
ţ́⊂⊃	PAINT STRIPING TRUCK OR OTHER OPERATING VEHICLE	VALUES SHOWN ABO	VE. ROAD DESIGNATIONS SHA		THE NJDOT STATE HIGHWAY			

DRIVEWAY E5-1(MOD) (R)

REGULATORY APPROACH SPEED OF TRAFFIC	RECOMMENDED SIGHT DISTANCE TO BEGINNING OF CHANNELIZING TAPERS		
	DESIRABLE		MINIMUM
	RURAL	URBAN	RURAL AND URBAN
MILES/HOUR	FEET	FEET	FEET
25	375	525	150
30	450	625	200
40	525	725	250
45	600	825	325
50	675	925	400
55	750	1025	475
60	875	1150	550
65	1000	1275	725

- DESIRABLE VALUES SHALL BE PROVIDED WHEREVER POSSIBLE. IF IT IS NOT FEASIBLE OR PRACTICAL TO PROVIDE DESIRABLE VALUES BECAUSE OF HORIZONTAL OR VERTICAL CURVATURE OR IF RELOCATION OF THE TAPER IS NOT POSSIBLE, THEN MINIMUM VALUES CAN BE APPLIED. WHEN MINIMUM VALUES ARE USED. SPECIAL ATTENTION SHOULD BE GIVEN TO THE USE OF SUITABLE TRAFFIC CONTROL DEVICES FOR PROVIDING ADVANCED WARNING OF THE CONDITIONS THAT ARE LIKELY TO BE ENCOUNTERED.
- TAPERS SHALL BE LOCATED TO MAXIMIZE THE VISIBILITY OF THEIR TOTAL LENGTH. NO CHANNELIZING TAPERS SHALL BE PLACED ACROSS AN INTERSECTING SIDE STREET. THE LOCATION OF CHANNELIZING TAPERS SHOULD BE ADJUSTED AS REQUIRED TO AVOID CROSSING A SIDE STREET.



CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS WITHIN WORK AREA AT ALL TIMES. CONTRACTOR SHALL CONTACT PROPERTY OWNERS 3 DAYS IN ADVANCE OF WORKING WITHIN INDIVIDUAL DRIVEWAYS.

TRAFFIC CONTROL AT DRIVEWAYS DETAIL



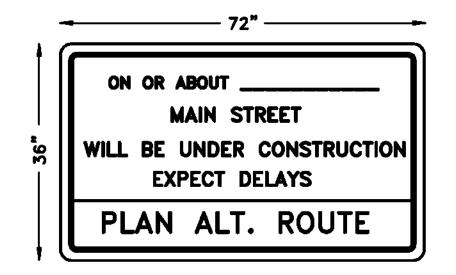
GENERAL NOTES:

1. DIMENSIONS, COLORS, AND DETAILS OF VARIOUS SIZE SIGNS, AND ACCESSORY PANELS TO PANELS TO FOLLOW STANDARDS IN THE CURRENT " STANDARD HIGHWAY SIGN PUBLICATION" AND THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS".

- 2. (S) REPRESENTS A SPECIAL SIZE SIGN.
- 3. LETTERS AND NUMERALS SHALL CONFORM TO THE CURRENT MANUAL, "STANDARDS ALPHABETS FOR
- HIGHWAY SIGNS" U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION.
- 4. THE CONTRACTOR SHALL OBTAIN THE APPROVAL OF THE ENGINEER FOR THE DISTANCE TO BE USED ON THE ADVANCE WARNING SIGNS, AND FOR THE SPEED LIMIT TO BE USED ON THE R2-1 SIGN.
- 5. DISTANCE LEGEND: SIGN NUMBER FOLLOWED BY LETTER & DISTANCE

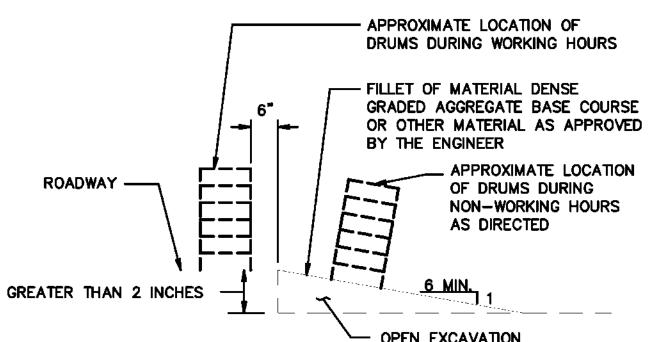
GENERAL NOTES FOR CONSTRUCTION WITHIN SIDEWALK AREAS

- 1. THE TYPICAL WORK AREA DETAIL FOR SIDEWALK AND CURB CONSTRUCTION SHALL ALSO APPLY TO THE CONSTRUCTION OF ALL OTHER WORK WITHIN THE EXISTING SIDEWALK AREA.
- 2. SIMULTANEOUS WORK ON ADJACENT BLOCKS IS PROHIBITED. CONTRACTOR SHALL COMPLETE ALL WORK, EXCLUDING MILLING & PAVING ASSOCIATED WITH FULL WIDTH RESURFACING ON ONE BLOCK BEFORE PROCEEDING WITH ANY WORK ON ADJACENT BLOCKS.
- 3. ALL PAVEMENT RESTORATION REQUIRED FOR CONSTRUCTION OF NEW CONCRETE CURB SHALL BE CONSTRUCTED PRIOR TO OPENING ANY BLOCK/WORK AREA TO TRAFFIC, EXCEPT FOR THE FINAL TOP COURSE OF SUPERPAVE SURFACE COURSE, MIX 9.5M64, 1.5" THICK. 4. MULTIPLE OPERATIONS MAY BE IN PROGRESS ON ONE SIDE OF THE STREET, BUT MUST BE SEPARATED BY AT LEAST TWO BLOCKS.
- 5. CONTRACTOR SHALL LIMIT THE AMOUNT OF DISTURBED AREA TO UNDER 5000 SF AT ANY ONE TIME, AND ALL AREAS SHALL BE STABILIZED AND SECURED AT THE COMPLETION OF THE DAILY WORK.
- 6. PLACEMENT OF DRUMS IN PARKING AREAS SHALL NOT ENCROACH EXISTING TRAVEL LANES DURING CONSTRUCTION ACTIVITIES ON ANY GIVEN BLOCK, EXCEPT FOR THE CONSTRUCTION OF DRAINAGE PIPE.
- 7. FOR WORK ON ANY GIVEN BLOCK, CONSTRUCTION OF ANY UNDERGROUND WORK WITHIN THE EXISTING TRAVEL LANES SHALL BE STAGED SUCH THAT DISRUPTION OF THE TRAVEL LANES IS MINIMIZED. LANE CLOSURES SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE STANDARD TRAFFIC CONTROL DETAILS CONTAINED HEREIN.
- 8. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL EXISTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION.
- 9. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS ON BELMONT AVENUE WITHIN THE PROJECT LIMITS DURING CONSTRUCTION.
- 10. BREAKAWAY BARRICADES SHALL BE PLACED END TO END ACROSS THE ENTIRE WIDTH OF THE SIDEWALK AT THE LIMITS OF THE WORK AREA. 11. THE CONTRACTOR SHALL PLACE "EMERGENCY-NO PARKING" SIGNS A MINIMUM OF 24 HOURS PRIOR TO CLOSING ANY BLOCK OR RESTRICTING ANY PARKING FOR ALL WORK AREAS.
- 12. ALL WORK AREAS SHALL BE EITHER OPENED FOR PUBLIC ACCESS OR SECURED TO THE SATISFACTION OF THE RESIDENT ENGINEER DURING NON-WORKING HOURS.
- 13. THE CONTRACTOR SHALL NOTIFY PROPERTY OWNERS, A MINIMUM OF 3 DAYS BEFORE COMMENCING WORK ON THAT BLOCK.



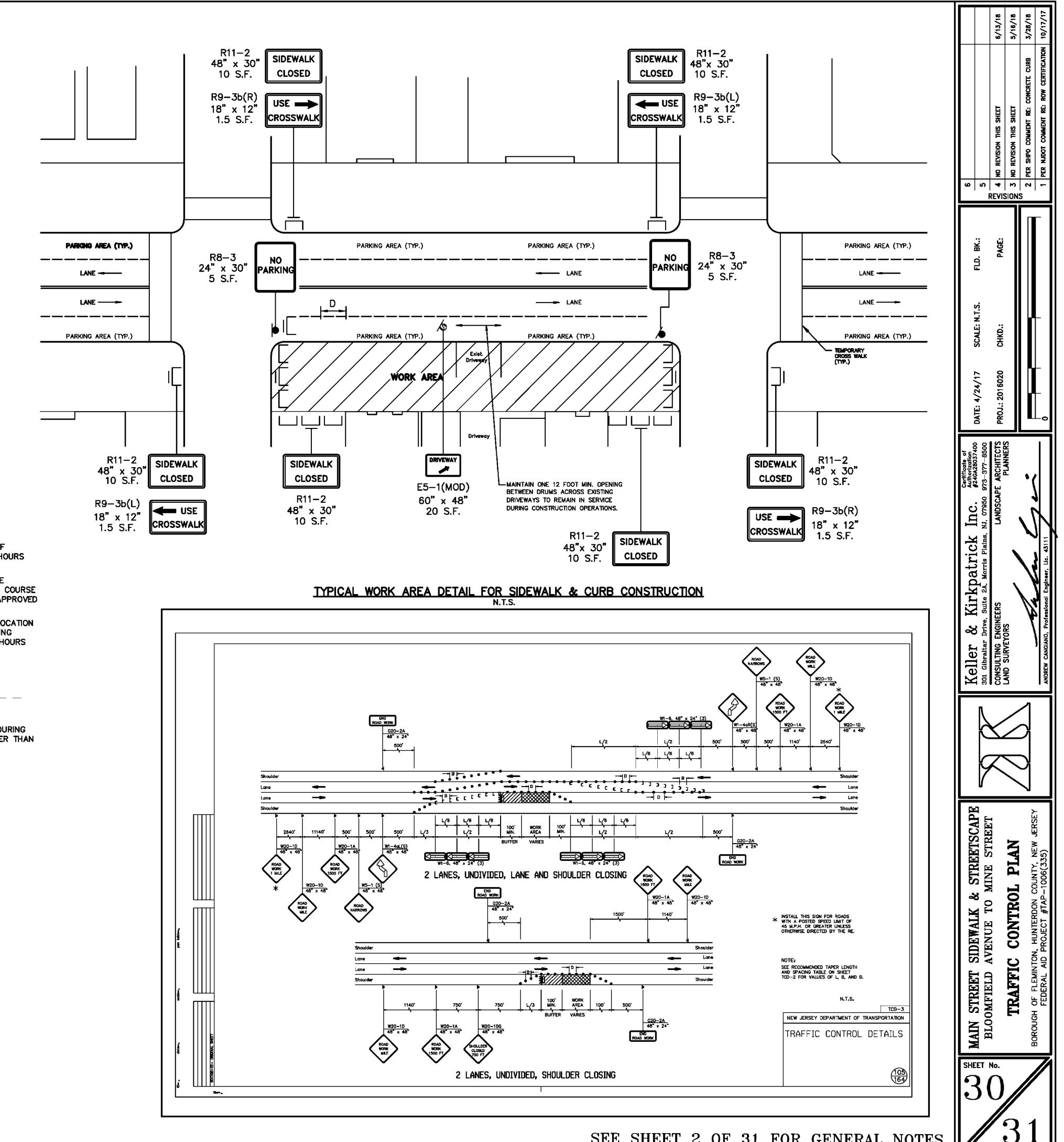
CONSTRUCTION ADVANCE WARNING SIGNS SHALL BE PLACED IF AND WHERE DIRECTED BY THE RESIDENT ENGINEER A MINIMUM OF 14 DAYS PRIOR TO COMMENCING WITH CONSTRUCTION.

CONSTRUCTION IDENTIFICATION SIGN 'A' N.T.S.

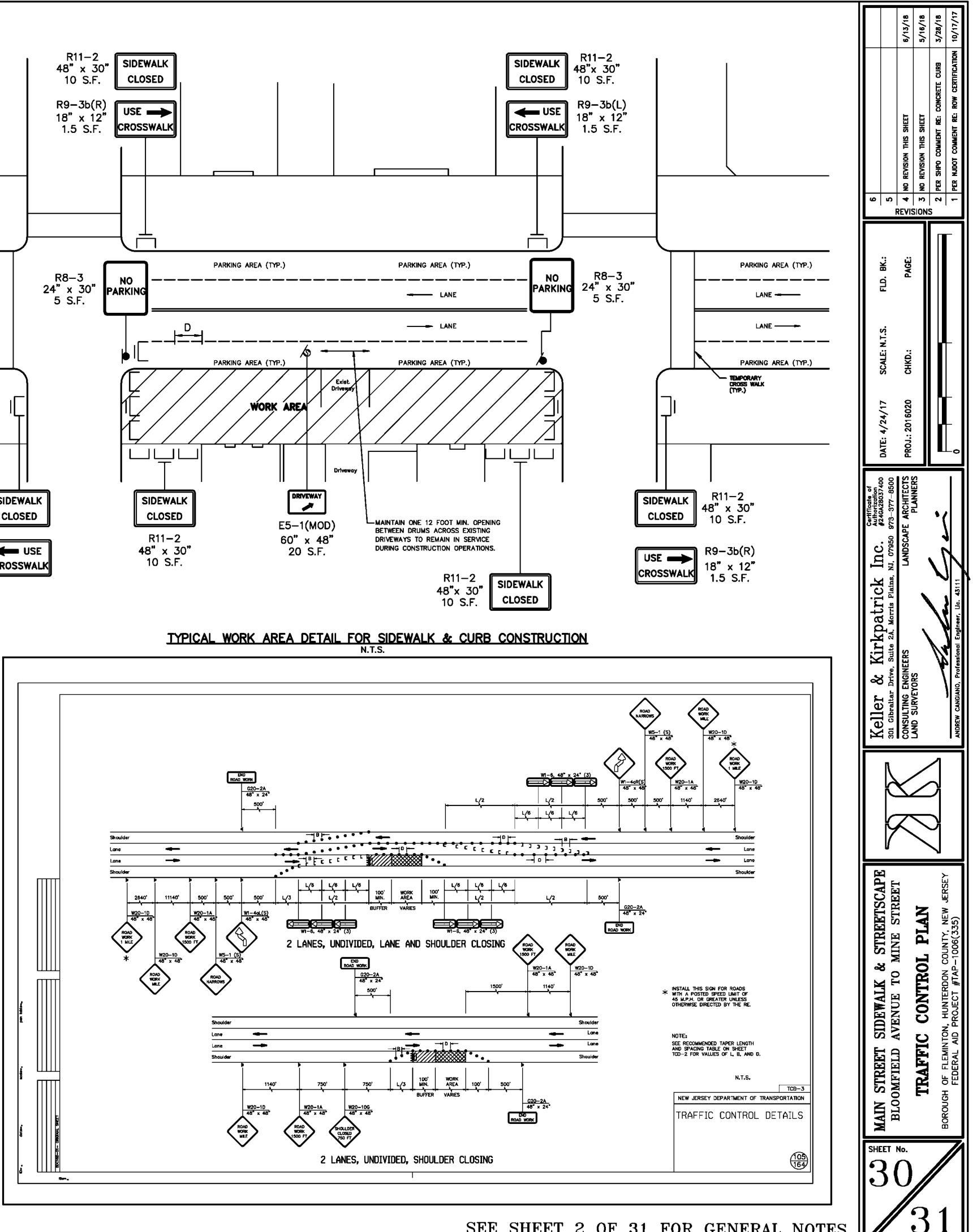


NOTE: ESCAPE RAMPS MUST BE CONSTRUCTED AND MAINTAINED DURING NON-WORKING HOURS WHERE A VERTICAL DROP OF GREATER THAN 2 IN EXISTS ADJACENT TO A TRAVELED LANE.





OPEN EXCAVATION



SEE SHEET 2 OF 31 FOR GENERAL NOTES